

**ALA WAI CANAL
Between Ala Wai Boulevard
and Ainakea Way
Honolulu
Honolulu County
Hawaii**

PHOTOGRAPHS

WRITTEN HISTORICAL AND DESCRIPTIVE DATA

**Historic American Engineering Record
National Park Service
U.S. Department of the Interior
1849 C Street, NW
Washington, DC 20240**

HISTORIC AMERICAN ENGINEERING RECORD

INDEX TO PHOTOGRAPHS

HAER No. HI-143

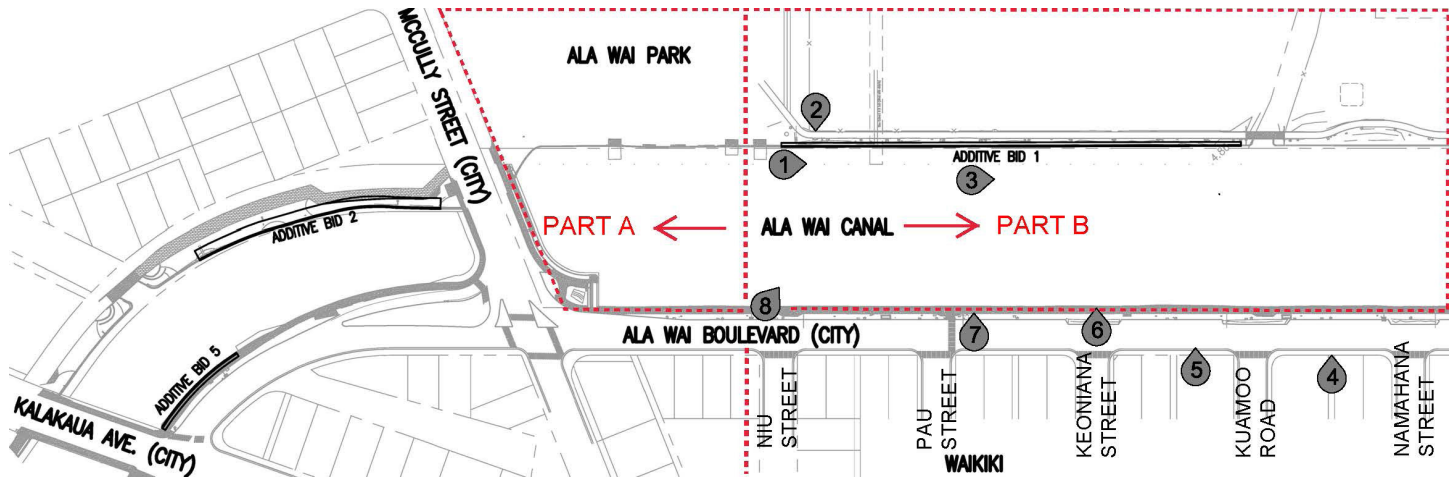
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David Franzen, Photographer, July 2, 2019

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PHOTO KEY — SITE PLAN



1 ALA WAI PHOTO KEY - SITE PLAN
SCALE: NOT TO SCALE

LEGEND



PHOTO KEY — SITE PLAN - Part A

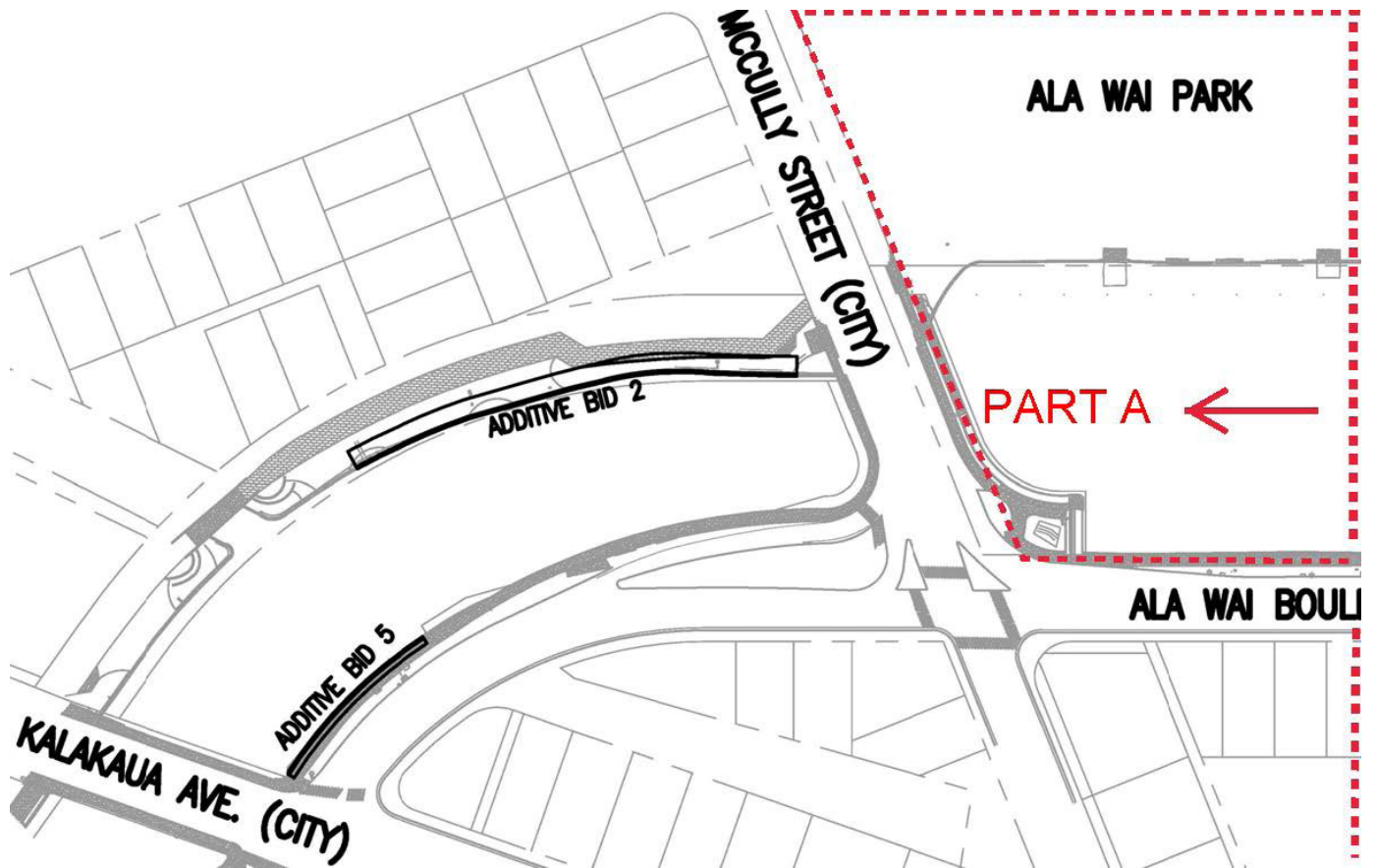
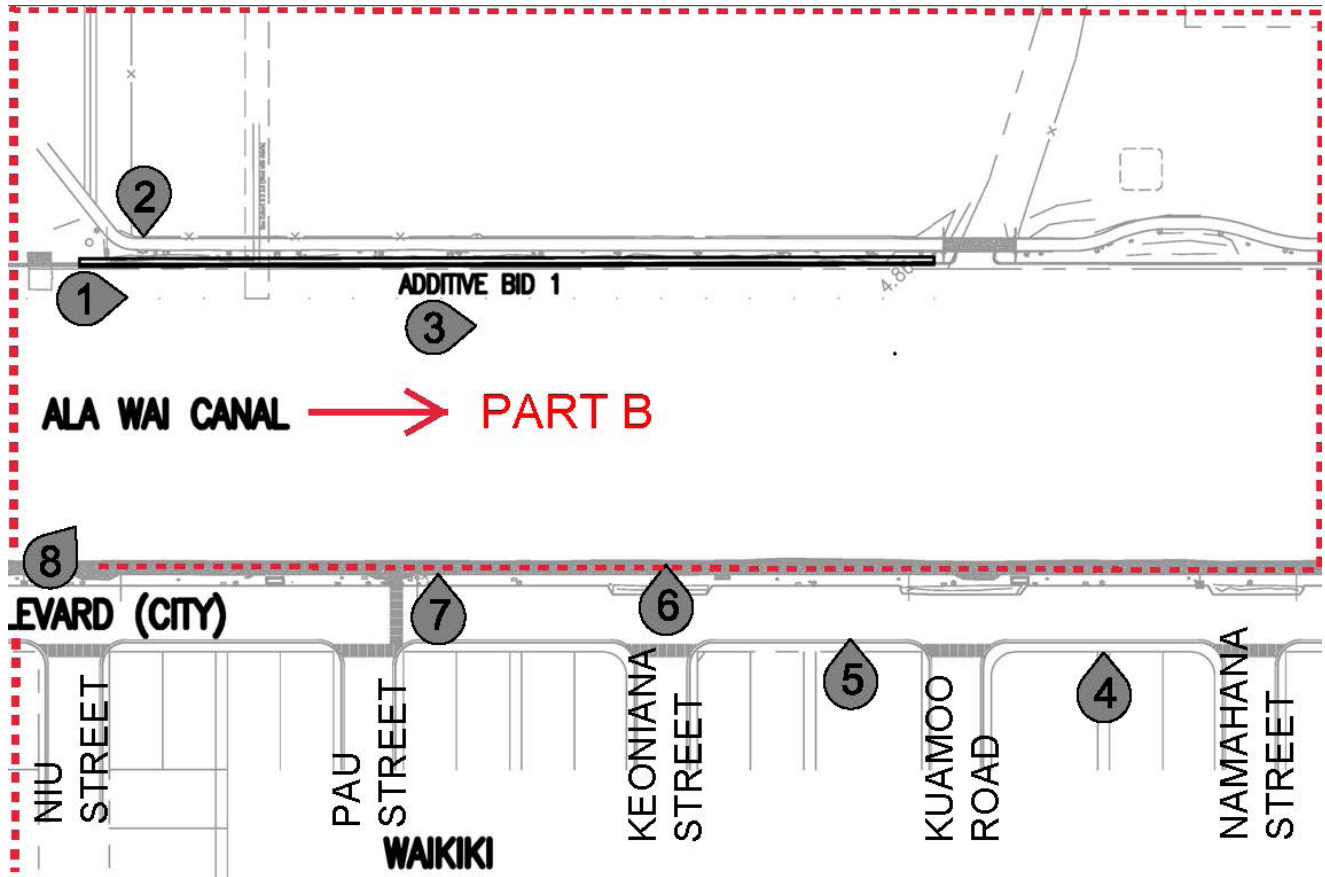


PHOTO KEY — SITE PLAN - Part B



HISTORIC AMERICAN ENGINEERING RECORD

ALA WAI CANAL

HAER No. HI-143

Location:	<p>Between Ala Wai Boulevard and Ainakea Way Honolulu, Honolulu County, Hawaii The canal is located between latitude: 21.287532, longitude: -157.841107 (west end) and latitude: 21.275136, longitude: -157.817733 (east end). This survey focuses on portion of the canal between McCully and Namahana Streets (21.287883, -157.832535 to 21.285519, -157.829770). These points were obtained on July 16, 2019, using Google Earth (WGS84). There is no restriction on their release to the public.</p>
Present Owner:	<p>State of Hawaii</p>
Present Occupants:	<p>Vacant</p>
Present Use:	<p>Waterway</p>
Significance:	<p>The Ala Wai Canal is associated with the development of Waikiki as the major visitor center on the island of Oahu. The canal diverted the waters of the Palolo, Manoa, and Makiki streams from Waikiki, eliminating the wetlands in the area, which allowed a more intense development of Waikiki. It also is a good example of a drainage canal constructed in Hawaii during the 1920s and 1930s. The lava rock walls were constructed in 1934 and substantially repaired in 1949. The lava rock walls are typical of their period in their workmanship and use of materials, as is the overall design of the canal.</p>
Description:	<p>The canal is a prominent landmark which demarcates the boundary between modern day Waikiki and the McCully, Moiliili, Kapahulu and Ala Moana districts of Honolulu.</p> <p>The Ala Wai Canal is a revetment type structure with a dredged bottom and lava rock walls reinforcing its <i>makai</i> (to the ocean side) bank, and concrete coated lava rock walls on its <i>mauka</i> (the mountain side) side. The canal is approximately two miles long, and is 150' to 250' wide. Originally the canal was dredged to a depth of 10' to 25'; however, today the average depth is between 6' and 10'. On the <i>mauka</i> side of the canal the Manoa-Palolo Ditch, Alanaio Stream, Kapahulu Ditch, and Makiki Ditch flow into the canal, which conveys these waters to the ocean at Ala Moana Park. Three bridges, at McCully Street, Kalakaua Avenue, and Ala Moana Boulevard, traverse the canal.</p> <p>The canal is comprised of two straight running segments which intersect at Kalakaua Avenue at a forty-five degree elbow. The shorter segment opens to the ocean at the Ala Wai Boat Harbor and is approximately</p>

2,460' long, and approximately 165' wide. The other segment runs from Kalakaua Avenue to Kapahulu Avenue and is approximately 7,710' long and approximately 250' wide. On the canal's *makai* side the Ala Wai Boulevard runs parallel to the canal for its entire length, as does a concrete sidewalk. Sixteen sets of concrete steps lead from the sidewalk down to the canal at intermittent points, primarily opposite streets intersecting with Ala Wai Boulevard. The *mauka* side of the canal is bordered by a golf course, school grounds and parks between Kapahulu Avenue and Kalakaua Avenue, and by a paved promenade shaded by Chinese banyan trees (*Ficus microcarpa*) between Kalakaua Avenue and Ala Moana Boulevard. The Ala Wai Yacht Club and Ala Wai Boat Harbor are at the mouth of the canal.

Historical Context:

For many centuries the lands *mauka* of present day Kalakaua Avenue were dotted by *loko* (fish ponds) and *kalo loi* (taro fields), contributing to the sustenance of the Hawaiian people.¹ During the late nineteenth century, much of the area was placed into rice cultivation. The Kanaha, Makiki, Manoa and Palolo streams and their tributaries drained the upland mountain areas providing waters to their valleys and the lowlands below. The waters from these streams dispersed over the broad plain, and were channeled into *auwai* (irrigation canals) by the Hawaiian population, or freely flowed through such streams as the Kalia, Pahoia, and Alanaio, meandering across the lowlands before eventually making their way to the ocean by way of the Kuekaunahi, Piinaio, and Apuakehau Streams (Fig. 3 and Fig. 4).

The Ala Wai Canal diverted the upland waters from Waikiki's ocean front area, eliminating existing pathways to the sea and redirecting the waters to the ocean. Bereft of water, the low lying *makai* lands were filled and developed to meet residential and commercial needs.² Following the example of draining and filling Waikiki's lands, the wetland areas *mauka* of the Ala Wai Canal gradually underwent a similar transformation, as the demand for urban lands expanded during the 1920s and 1930s.³ Thus the Ala Wai Canal made possible the development of twentieth century Waikiki as a residential area and ultimately the center of Oahu's visitor industry.

Waikiki's wetland geography remained into the opening decades of the twentieth century, with approximately 85% of modern Waikiki laying under water, used primarily for the cultivation of rice and taro or as fish and duck ponds. In 1906, a thirty-six page report, penned by Lucius Pinkham, the president of the Territorial Board of Health, declared the "swamps" of Waikiki to be "deleterious to the public health," as they were a breeding ground for mosquitoes.

1 Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986: 84-88.

2 Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986: 93-209.

3 Mason Architects, *Historic Structures Inventory Survey of the Ala Wai Watershed*. Honolulu: 2009: 20-22.

The plan proposed to “reclaim” the wetlands of Waikiki by building a canal to divert the streams flowing into the area and filling the resulting lowlands. The plan was shelved and nothing more was heard of it until 1913 when president Woodrow Wilson appointed Pinkham to be the territorial governor of Hawaii.⁴

The governor resurrected his earlier plan and the Department of Public Works studied the plan in earnest. The 1918 Legislature appropriated \$100,000 for the excavation of the canal, and over the course of the next two years, the Territory procured the 161 acres of the proposed canal’s 800’ right of way. Many of the lands were acquired by condemnation under an 1896 law. This law authorized Hawaii’s Board of Health to judge whether any land was unsanitary and to require the owners to take the necessary steps to improve the land, usually by filling it. For the wetland farmers in Waikiki it essentially meant the eradication of their livelihood. In addition, if under the law, land owners were financially unable to fill the land, or unwilling to do so, the government could undertake the improvement and cover the cost through a lien placed on the property. Through this process many wetland farmers in Waikiki lost not only their livelihood but also their land.⁵

By June 1920, 85% of the land required for the building of the canal had been acquired and bids to dredge the canal were solicited. In December 1920, the Hawaiian Dredging Company, a subsidiary of the Dillingham-owned Oahu Railway & Land Company, was awarded a contract to build the canal.⁶ The 1921 Legislature passed Act 221, which confirmed the boundaries of the reclamation project and established a commission to plan for boulevards, streets and parks within the reclamation district.⁷

A relatively straight-forward construction project, the canal was built between 1921 through 1928 as a realigned channel with natural banks.⁸ By mid-1923 the Hawaiian Dredging Company’s dredge, the “Kewalo”, had made its way almost 6,500’ towards Kapahulu Avenue, cutting a channel approximately 135’ wide and 10’ - 20’ deep. The Superintendent of Public Works, Lyman H. Bigelow, reported that the canal “has now intercepted Apuakehau Stream which flowed by the Outrigger Club and all the filthy waters which previously flowed on to this fine swimming beach have been diverted and now flow out to the sea by way of the canal.”⁹

By mid-1924, the canal was 150’ wide and had been dredged “its en-

4 Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986: 86-91.

5 Nakamura, Barry, *The Story of Waikiki and the ‘Reclamation’ Project*. Unpublished Masters Thesis, University of Hawaii, 1979.

6 Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986: 88.

7 Session Laws, 1921.

8 Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986: 86-92.

9 *Annual Reports of the Territorial Department of Public Works*, 1918-1955, (1923), 12.

tire length” to Kapahulu Avenue.¹⁰ Due to a lack of funds the Diamond Head end of the canal, called for in Lucius Pinkham’s original proposal, was put on hold until “some later date, when funds are made available.”¹¹ This portion of the canal was in fact never completed.

While moneys were not available to complete the canal to its full length, the 1923 Legislature had provided funds to widen the canal an additional 100’. Ostensibly this widening was, “desired in case additional filling material was required and also to develop the canal for recreational purposes, such as boat racing, water carnivals, etc.”¹² In fact, the expansion primarily provided fill to reclaim the McCully tract, a vast area of pond fields and fishponds, owned by Dillingham, which lay on the *mauka* side of the canal.¹³ By mid-1927 the filling of the McCully tract was completed and the canal was 250’ wide almost to Kapahulu Avenue. In 1928 the “Kewalo” exited the canal and the construction of the Ala Wai was thus completed.¹⁴

The canal acquired its name in 1925 when the City Planning Commission requested that citizens of Honolulu submit suitable names for the re-naming of the Waikiki drainage canal. Jennie Wilson, wife of Mayor John Henry Wilson, suggested the name *Ala Wai*, Hawaiian for “waterway.”^{15,16}

The absence of side walls was quickly recognized as a problem and in 1921 the Territorial Legislature passed Act 220, which recognized the Board of Health’s establishment of a Drainage District for the Waikiki area as “necessary for the proper drainage and sanitation” of Waikiki.¹⁷ The law also appropriated \$600,000 to construct side walls and a floor where needed, and authorized the Territorial Department of Public Works to assess all property owners within the drainage district in order to reimburse the governmental appropriation. Property owner opposition to this measure immediately arose, challenging the Territory’s prerogative to assess owners for the construction of the revetments. Rather than have this issue settled in the courts, the 1925 Legislature passed Act 149 repealing Act 220. As a result the Ala Wai Canal remained a completely unlined channel for the first few years of its existence.¹⁸

As early as January 1931 the Shade Tree Commission expressed concern about the erosion of the Ala Wai’s banks, which were, “being eaten away by waters of the canal, assisted by waves from the motor boats which ply

¹⁰ *Annual Reports of the Territorial Department of Public Works*, 1918-1955, (1924), 10.

¹¹ *Ibid.*

¹² *Annual Reports of the Territorial Department of Public Works*, 1918-1955, (1923), 13.

¹³ *Annual Reports of the Territorial Department of Public Works*, 1918-1955, (1925), 7.

¹⁴ Hibbard, Don and Franzen, David, *The View from Diamond Head, Royal Residence to Urban Resort*. Honolulu: Editions Limited, 1986.

¹⁵ “Wanted Hawaiian Name for Waikiki Drainage Canal.” Can You Help?” *Honolulu Advertiser* (Honolulu), March 22, 1925, 8.

¹⁶ “Do You Like Ala Wai as Name of Waikiki Canal? Planners have Chosen It.” *Honolulu Advertiser* (Honolulu), April 5, 1925, 12.

¹⁷ Session Laws, 1921.

¹⁸ Mason Architects, *Historic Structures Inventory Survey of the Ala Wai Watershed*. Honolulu: 2009, p11.

back and forth along the waterway.”¹⁹ The erosion not only endangered the trees planted along the banks, but also the Ala Wai Boulevard, which as late as 1950, the editor of the *Honolulu Advertiser* viewed as “a rock sprinkled dirt road that would disgrace a modern American barnyard.”²⁰ Territorial Superintendent of Public Works, Lyman H. Bigelow informed the Shade Tree Commission that the department had no funds to undertake repair work, and help should be sought from the City and County Board of Supervisors. No money was forthcoming until 1934, when federal Civil Works Administration (CWA) funds were made available, and the City & County constructed lava rock revetments to protect the banks, as the government had to develop “projects on which men out of employment could be worked.”²¹

Construction on the lava rock walls commenced in late January 1934, thanks to an infusion of CWA moneys,²² however, after the expenditure of \$7,379 of these funds, as well as \$1,333.20 of City & County moneys, the CWA funds for the Ala Wai improvement project were discontinued before March 11, 1934,²³ as this federal program was only designed as a stop-gap measure to provide the unemployed with sustenance during the winter months. At this point approximately one third of the canal had been lined with walls.²⁴

The job was completed when Federal Emergency Relief Act (FERA) funds became available in April, 1934.²⁵ Both these federal funds and the CWA moneys could only be expended on salaries, with the local governments having to contribute the cost of materials, equipment, and overhead. In part, materials for the walls was obtained from another FERA project which was busy removing coral from the ocean waters in front of Waikiki Beach.²⁶

By 1949 the CWA/FERA constructed revetments were in need of repair. The *makai* walls began to break down during World War II, with the walls in many places bulging out and large rocks falling into the water. Holes appeared in the concrete cap. Following the war, small sections and then large portions of the wall crumbled. In addition, in a number of places, the ground behind the wall sank. In early 1949, the City & County Public Works Department commenced repairing the wall, beginning at the Kapahulu end of the canal. Near the end of summer, materials allocated for the project had been exhausted and work on the wall stopped with the assigned County

19 “Erosion on Ala Wai Becoming Real Problem.” *Honolulu Advertiser* (Honolulu), January 7, 1931, 1.

20 “Civic Disgrace Along Ala Wai.” *Honolulu Advertiser* (Honolulu), March 31, 1950, 8.

21 *Annual Reports of the Honolulu Department of Public Works, 1933-1955*, 4.

22 “County Asks CWA Aid in \$211,782 Public Projects.” *Honolulu Advertiser* (Honolulu), January 17, 1934, 1-4.

23 “14 CWA Projects Underway in Honolulu, 6 Completed.” *Honolulu Advertiser* (Honolulu), March 21, 1934, 2.

24 “New Park Work Speeding, Old Parks Suffer.” *Honolulu Advertiser* (Honolulu), March 11, 1934, 2.

25 “Hawaii Gains Improvements Through FERA.” *Honolulu Star Bulletin* (Honolulu), April 14, 1934, 3.

26 “Toes of Waikiki Bathers Safe; Coral is Removed.” *Honolulu Star Bulletin* (Honolulu), June 8, 1934, 1.

laborers placed to work resurfacing County roads.²⁷ At this point about 3,000' of the *makai* wall had been repaired, as well as portions of the *mauka* wall.²⁸

Advocating for a resumption of the wall repairs, the Republican-oriented *Honolulu Advertiser* noted,

The Ala Wai is a vital unit in scenic Honolulu, and merits continuing official attention. One of the principal problems in maintaining the canal's *makai* wall arises from the manner in which it was first constructed. The work was done by "depression labor" in the era of Harry Hopkins' boondogglers and the foundation is faulty. It seems to an onlooker that the wise course to be followed now would be to undertake a complete reconstruction of the wall according to the best engineering practice.

To rebuild the whole wall would be an extensive project, possibly too costly to be undertaken in its entirety at one time. But it could be rebuilt soundly by units, and this probably is the course that Engineer Sinclair contemplates. Mere patch work is not enough, and temporary construction, however well it may appear for a time, is extravagant in the long run.²⁹

Work on the canal walls was resumed in June 1950 and by the end of 1950 the *makai* wall was completed as far as Kalakaua Avenue.³⁰ Simultaneously, on August 24, 1950, E. E. Black was named the low bidder for a project to extend Ala Wai Boulevard beyond Kalakaua Avenue towards Ala Moana Boulevard. This project's scope included paving, curbing, sidewalks, water, drainage, and street lights, as well as repair of the canal wall.³¹ The handsome segmental arch balustrade was the result of E.E Black's endeavors.

Thanks to a 1953 Legislative appropriation, further work was undertaken on the canal's walls by the Territorial Department of Public Works. In 1954 a contract was let to Pacific Construction Company to repair the *mauka* wall as well as the walls at the mouth of the canal. In addition to replacing missing rocks and patching the concrete coping that topped the wall, they added a concrete liner and replaced the earth fill behind the *mauka* wall.³²

27 "The Mauka Ala Wai Canal Wall, With and Without Repair." *Honolulu Advertiser* (Honolulu), October 23, 1949, 2.

28 Ibid.

29 "Ala Wai Canal Improvements." *Honolulu Advertiser* (Honolulu), October 27, 1949, 8.

30 Cited in the following sources: "Outdoor Circle Getting Action in Repairs of Ala Wai Walls." *Honolulu Star Bulletin* (Honolulu), May 9, 1950, 11; "Repairs Started on Ala Wai Canal Wall." *Honolulu Star Bulletin* (Honolulu), June 30, 1950, 6; "Ala Wai Canal Wall Gets Face Lifting." *Honolulu Advertiser* (Honolulu), August 28, 1950, 9; "Ala Wai Canal Wall Repairs Progressing." *Honolulu Star Bulletin* (Honolulu), October 23, 1950, 19.

31 "\$96,000 Bid on Ala Wai Project." *Honolulu Advertiser* (Honolulu), August 25, 1950, 10.

32 "Ala Wai Wall Repairs Are Progressing." *Honolulu Advertiser* (Honolulu), June 13, 1954, 27.

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Annual Reports of the Territorial Department of Public Works, 1918-1955.

Google Earth (WGS84). Latitude, Longitude, (accessed on March 24, 2016).

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Honolulu Advertiser (Honolulu). "Ala Wai Canal Wall Gets Face Lifting.", August 28, 1950, 9.

Honolulu Star Bulletin (Honolulu). "Ala Wai Canal Wall Repairs Progressing.", October 23, 1950, 19.

Interior Department Book 22, held at the Hawaii State Archives.

M.D. Monsarrat [map]. July 10, 1897. Scale not given. "Hawaii State Survey Division".

Kuekaunahi, Piinaio, and Apukehau Streams [map]. date unknown. Scale not given. "Hawaii State Survey Division".

Mason Architects, *Historic Structures Inventory Survey of the Ala Wai Watershed.* Honolulu: 2009.

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Pinkham, Lucius, *Hawaii Board of Health Report: The Reclamation of the Waikiki District.* Honolulu: 1906.

Session Laws, 1921.

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"14 CWA Projects Underway in Honolulu, 6 Completed." *Honolulu Advertiser* (Honolulu), March 21, 1934, 2.

"\$96,000 Bid on Ala Wai Project." *Honolulu Advertiser* (Honolulu), August 25, 1950, 10.

"Ala Wai Canal Improvements." *Honolulu Advertiser* (Honolulu), October 27, 1949, 8.

"Ala Wai Wall Repairs Are Progressing." *Honolulu Advertiser* (Honolulu),- June 13, 1954, 27.

"Ala Wai Canal Wall Work Due in the Spring." *Honolulu Advertiser* (Honolulu), October 23, 1949, 2.

"Civic Disgrace Along Ala Wai." *Honolulu Advertiser* (Honolulu), March 31, 1950, 8.

"Complaint from the Ala Wai." *Honolulu Advertiser* (Honolulu), October 14, 1949, 6.

"County Asks CWA Aid in \$211,782 Public Projects." *Honolulu Advertiser* (Honolulu), January 17, 1934, 1-4.

"Do You Like Ala Wai as Name of Waikiki Canal? Planners have Chosen It." *Honolulu Advertiser* (Honolulu), April 5, 1925, 12.

"Erosion on Ala Wai Becoming Real Problem." *Honolulu Advertiser* (Honolulu), January 7, 1931,1.

"Extension of Ala Wai Blvd Gets Final OK." *Honolulu Advertiser* (Honolulu), July 26, 1950, 13.

Honolulu Advertiser (Honolulu). "Ala Wai Canal Wall Gets Face Lifting.", August 28, 1950, 9, (accessed image July 2019).

"Mayor, Board Urged to Complete Ala Wai Wall." *Honolulu Advertiser* (Honolulu), May 27, 1951, 32.

"New Park Work Speeding, Old Parks Suffer." *Honolulu Advertiser* (Honolulu), March 11, 1934, 2.

"Redevelopment Project OK'd by City Board." *Honolulu Advertiser* (Honolulu), July 4, 1950, 11.

"Repair Work Starts on Ala Wai Retaining Wall." *Honolulu Advertiser* (Honolulu), June 30, 1950,15.

"The Ala Wai Wall Will Be Repaired, Trees Planted." *Honolulu Advertiser* (Honolulu), May 7, 1950, 30.

"The Mauka Ala Wai Canal Wall, With and Without Repair." *Honolulu Advertiser* (Honolulu), October 23, 1949, 2.

“Three Projects Completed.” *Honolulu Advertiser* (Honolulu), September 13, 1936, 1.

“WPA Employment Service Provides Many Jobs Here.”
Honolulu Advertiser (Honolulu), September 15, 1936, 1.

“Wanted Hawaiian Name for Waikiki Drainage Canal.” Can You Help?”
Honolulu Advertiser (Honolulu), March 22, 1925, 8.

Honolulu Star Bulletin

“\$225,000 Planned for Ala Wai Blvd.” *Honolulu Star Bulletin* (Honolulu), March 6, 1949, 20.

“About Repairs to the Ala Wai Wall.” *Honolulu Star Bulletin* (Honolulu), October 18, 1949, 8.

“Ala Wai Canal Wall Repairs Progressing.” *Honolulu Star Bulletin* (Honolulu), October 23, 1950, 19.

“City’s neglected Areas Becoming Beautiful Parks.” *Honolulu Star Bulletin* (Honolulu), January 27, 1934, 5.

“Deplorable Conditions on the Ala Wai.” *Honolulu Star Bulletin* (Honolulu), October 9, 1951, 8.

“Hawaii Gains Improvements Through FERA.” *Honolulu Star Bulletin* (Honolulu), April 14, 1934, 3.

“Outdoor Circle Asks for Rehabilitation of Ala Wai Canal.” *Honolulu Star Bulletin* (Honolulu), April 1, 1953, 6.

“Outdoor Circle Getting Action in Repairs of Ala Wai Walls.” *Honolulu Star Bulletin* (Honolulu), May 9, 1950, 11.

“Plans on Work for Honolulu’s Jobless Drawn.” *Honolulu Star Bulletin* (Honolulu), March 8, 1932, 3.

“Repairs Started on Ala Wai Canal Wall.” *Honolulu Star Bulletin* (Honolulu), June 30, 1950, 6.

“Toes of Waikiki Bathers Safe; Coral is Removed.” *Honolulu Star Bulletin* (Honolulu), June 8, 1934, 1.

Project Information: The following documentation was prepared in response to the proposal to repair a portion of the Ala Wai canal's *mauka* wall, located between McCully Street and the Alanaio Stream. The purpose of this documentation is to historically record the history of the canal and its historic materials and design. The property is listed in the Oahu State Register of Historic Places. The State Historic Preservation Division recommended that HAER documentation be completed as a means of mitigating the loss of one portion of this historic property.

Don J. Hibbard, Ph.D., who meets the Secretary of the Interior's Professional Qualifications in architectural history, and Cathy Zuniga prepared the written and historic portions of the report. The large-format photographs were taken by David Franzen of Franzen Photography.

Prepared By: Don Hibbard, Architectural Historian
Fung Associates, Inc.
1833 Kalakaua Avenue, Suite 1008
Honolulu, HI 96815

Date of Report: August 05, 2019

Fig. 1: Map of Honolulu, Hawaii (Source: Fung Associates, Inc.)

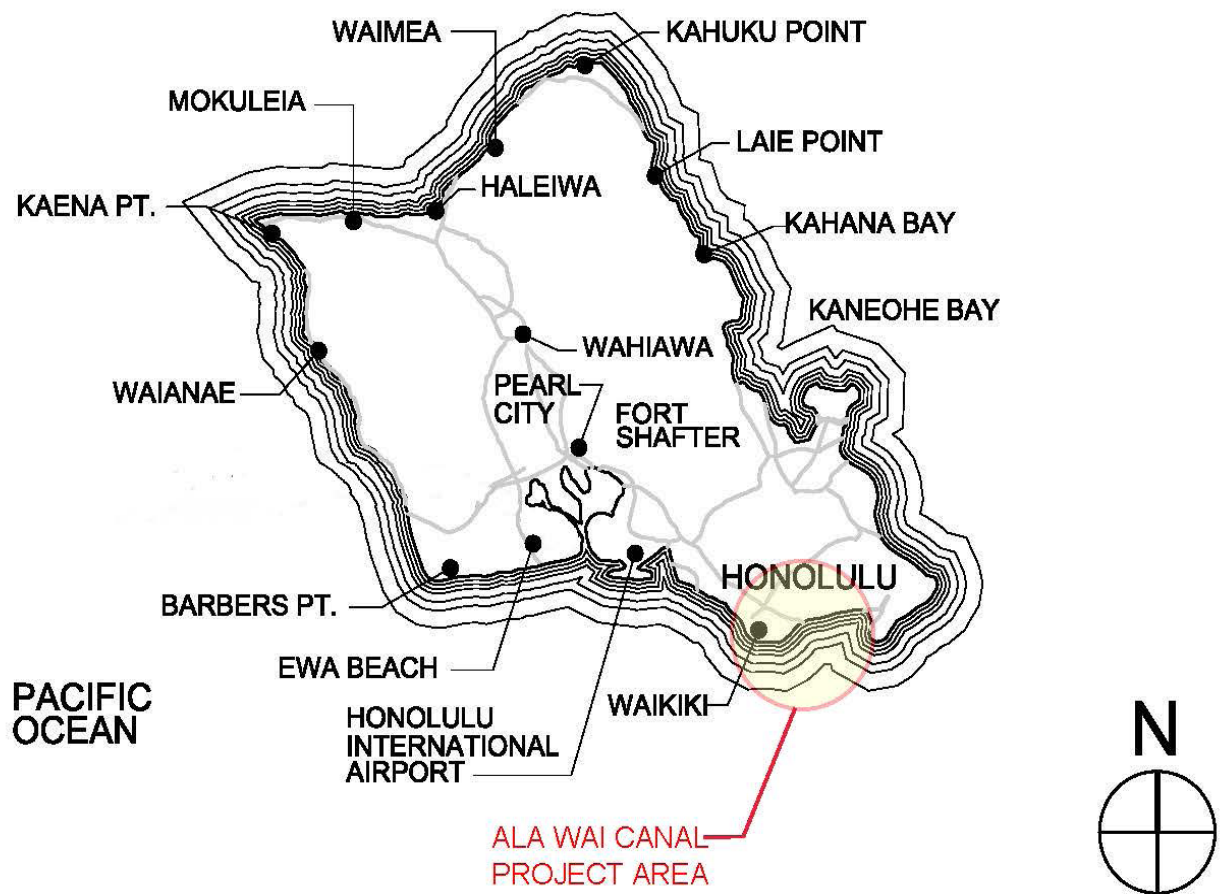


Fig. 2: U.S.G.S. Map, U.S.G.S. Honolulu Quadrangle 2017 (7.5 Minute Series) NAD83.



(Source: Hawaii State Survey Division)

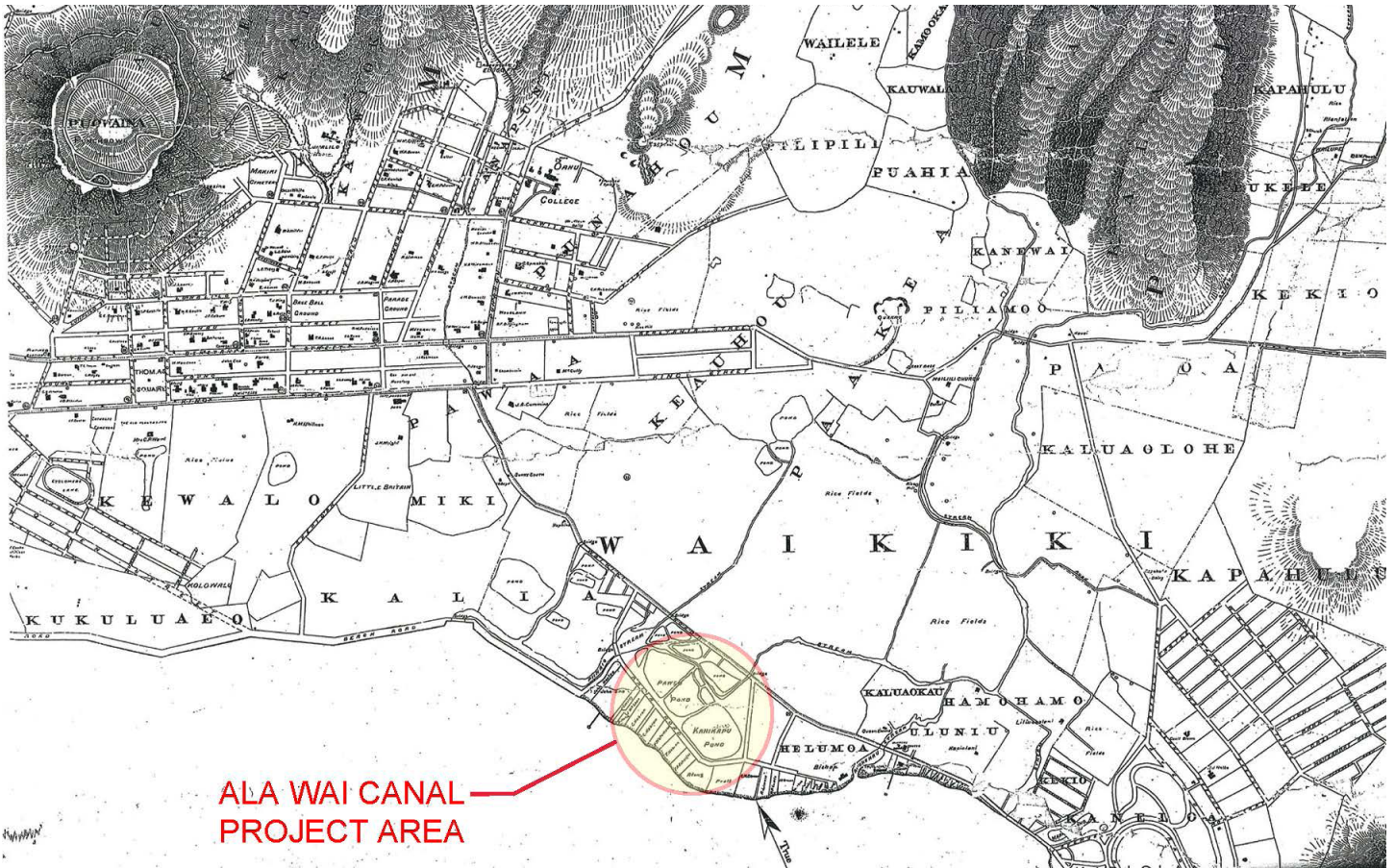
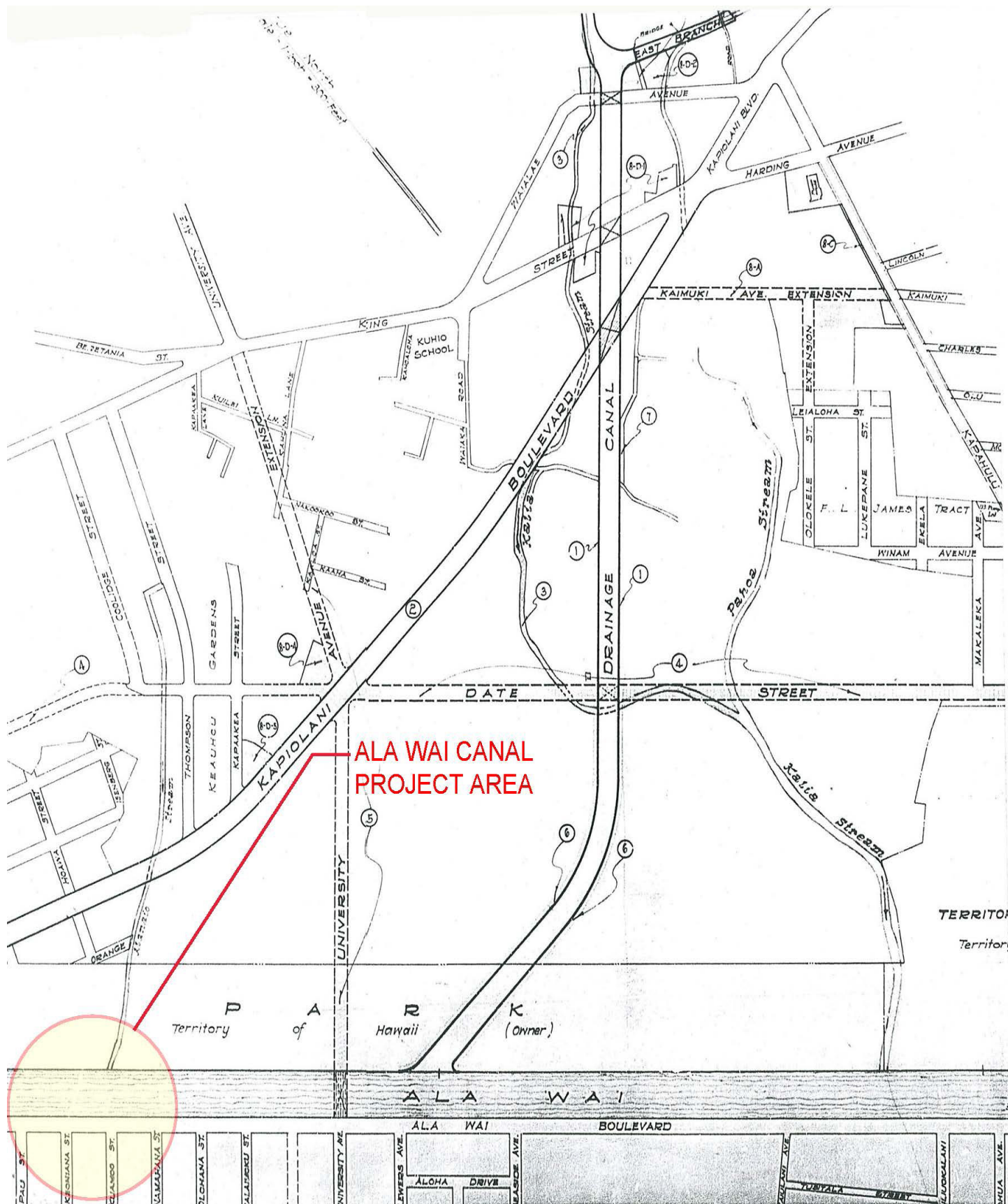


Fig. 4: Map of Kuekaunahi, Piinaio, and Apukehau Streams
(Source: Hawaii State Survey Division)



FIELD NOTES: HISTORIC IMAGES

Unclear copyright status -- not for reproduction.

Fig. 5: "Ala Wai Canal Gets Face Lifting" (August 28, 1950)
(Source: Honolulu Advertiser) Not to be used for commercial purposes.



ALA WAI CANAL WALL GETS FACE LIFTING—The stone and concrete retaining wall on the makai side of the Ala Wai canal, and paralleling the Ala Wai Blvd., long fallen to pieces in many sections, and long an eyesore, is currently getting a face-lifting that promises to be permanent. Pictured are city-county workmen installing a new concrete base preparatory to restoring the wall. Ala Wai residents now hope that the strip between the wall and the highway will be landscaped in keeping with the many coconut trees planted there years ago. (Advertiser photo.)

Unclear copyright status -- not for reproduction.

Fig. 6: "Ala Wai Canal Wall Repairs are Progressing" (October 23, 1950)
(Source: Honolulu Star Bulletin) Not to be used for commercial purposes.



ALA WAI CANAL WALL REPAIRS ARE PROGRESSING with city-county work crews expecting to complete the project up to Kalakaua bridge by Christmas. The mayor and supervisors recently assigned an additional \$10,000 for the job, making a total allocation of \$30,000 for the wall's rehabilitation. Photo shows Y. Kurano, foreman, top left, with his crew fixing the wall near Namahana St. The wall extends about 7,500 feet from Kapahulu to Kalakaua, according to City-County Engineer K. A. Sinclair. Repairs have been made from Kapahulu to a point beyond Namahana, or about 1,500 feet walkiki of Kalakaua.