

United States Department of the Interior
National Park Service

National Register of Historic Places Registration Form

This form is for use in nominating or requesting determinations for individual properties and districts. See instructions in How to Complete the National Register of Historic Places Registration Form (National Register Bulletin 16A). Complete each item by marking "x" in the appropriate box or by entering the information requested. If any item does not apply to the property being documented, enter "N/A" for "not applicable." For functions, architectural classification, materials, and areas of significance, enter only categories and subcategories from the instructions. Place additional entries and narrative items on continuation sheets (NPS Form 10-900a). Use a typewriter, word processor, or computer, to complete all items.

1. Name of Property

historic name Pu'u'ōpae Bridge

other names/site number Kalama Stream Bridge, Kapaa Homesteads Bridge #2

2. Location

street & number Pu'u'ōpae Rd., between Kalama & Kīpapa Rds. not for publication

city or town Kapa'a vicinity

state Hawai'i code HI county Kaua'i code 007 zip code 96746

3. State/Federal Agency Certification

As the designated authority under the National Historic Preservation Act, as amended, I hereby certify that this nomination request for determination of eligibility meets the documentation standards for registering properties in the National Register of Historic Places and meets the procedural and professional requirements set forth in 36 CFR Part 60. In my opinion, the property meets does not meet the National Register Criteria. I recommend that this property be considered significant nationally statewide locally. (See continuation sheet for additional comments.)

Signature of certifying official

Date

State or Federal Agency or Tribal government

In my opinion, the property meets does not meet the National Register criteria. (See continuation sheet for additional comments.)

Signature of certifying official

Date

State or Federal Agency or Tribal government

4. National Park Service Certification

I hereby certify that this property is:

- entered in the National Register
 - See continuation sheet.
- determined eligible for the National Register
 - See continuation sheet.
- determined not eligible for the National Register
- removed from the National Register
- other (explain): _____

Signature of Keeper

Date of Action

Joe
Edson Beall

5/25/05

5. Classification

Ownership of Property

(Check as many boxes as apply)

- private, public-local, public-State, public-Federal

Category of Property

(Check only one box)

- building(s), district, site, structure, object

Number of Resources within Property

(Do not include previously listed resources in the count.)

Table with columns for Contributing and Noncontributing resources, including counts for buildings, sites, structures, objects, and Total.

Name of related multiple property listing

(Enter "N/A" if property is not part of a multiple property listing.)

N/A

Number of contributing resources previously listed in the National Register

0

6. Function or Use

Historic Functions

(Enter categories from instructions)

Transportation: road-related

Current Functions

(Enter categories from instructions)

Transportation: road-related

7. Description

Architectural Classification

(Enter categories from instructions)

No style

Materials

(Enter categories from instructions)

foundation Concrete-encased steel walls roof other Masonry (abutments)

Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)

8. Statement of Significance

Applicable National Register Criteria

(Mark "X" in one or more boxes for the criteria qualifying the property for National Register listing)

- Criteria A, B, C, D with checkboxes and descriptions.

Criteria Considerations

(Mark "X" in all the boxes that apply.)

- Criteria A, B, C, D, E, F, G with checkboxes and descriptions.

Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

Areas of Significance

(Enter categories from instructions)

- Social history, Exploration/settlement, Community planning and development, Transportation, Commerce.

Period of Significance

1915-1936

Significant Dates

N/A

Significant Person

(Complete if Criterion B is marked above)

N/A

Cultural Affiliation

Undefined

Architect/Builder

Moragne, Joseph H., County Engineer and Road Supervisor

9. Major Bibliographical References

Bibliography (Cite the books, articles, and other sources used in preparing this form on one or more continuation sheets.)

See Continuation Sheets, page 14.

Previous documentation on file (NPS)

- preliminary determination of individual listing (36 CFR 67) has been requested.
previously listed in the National Register
previously determined eligible by the Natl Register
designated a National Historic Landmark
recorded by Historic American Buildings Survey
recorded by Historic American Engineering Record

Primary Location of Additional Data

- State Historic Preservation Office
Other State agency
Federal agency
Local government Kaua'i County
University Garden Island microfilm
Other

Name of repository: County Clerk's office; Kaua'i Community College

10. Geographical Data

Acreeage of Property ___ Less than one acre _____

UTM References

(Place additional UTM references on a continuation sheet) North American Datum, 1983; USGS Kapaa Quadrangle, 1996

1	/4/N/	/4/6/1/5/1/1/	/2/4/4/0/4/3/9/	3	/ / /	/ / /	/ / /	/ / /
	Zone	Easting	Northing		Zone	Easting	Northing	
2	/ / /	/ / /	/ / /	4	/ / /	/ / /	/ / /	/ / /

See continuation sheet

Verbal Boundary Description

(Describe the boundaries of the property on a continuation sheet.)

Boundary Justification

(Explain why the boundaries were selected on a continuation sheet.)

11. Form Prepared By

name/title _____ Patricia L Griffin _____

organization _____ date _____ August 9, 2004 _____

street & number _____ 6524 Kalama Rd. _____ telephone _____ 808.639.1019 _____

city or town _____ Kapa'a _____ state ___ HI ___ zip code _____ 96746 _____

Additional Documentation

Submit the following items with the completed form:

Continuation Sheets

Maps

A USGS map (7.5 or 15 minute series) indicating the property's location.

A sketch map for historic districts and properties having large acreage or numerous resources.

Photographs

Representative **black and white** photographs of the property.

Additional items

(Check with the SHPO or FPO for any additional items)

Property Owner

(Complete this item at the request of the SHPO or FPO.)

name _____ County of Kaua'i, ATTN: The Honorable Bryan Baptiste, Mayor _____

street & number _____ 4444 Rice St., Suite 235 _____ telephone _____ 808.241.6300 _____

city or town _____ Lihu'e _____ state ___ HI ___ zip code _____ 96766 _____

Paperwork Reduction Act Statement: This information is being collected for applications to the National Register of Historic Places to nominate properties for listing or determine eligibility for listing, to list properties, and to amend existing listings. Response to this request is required to obtain a benefit in accordance with the National Historic Preservation Act, as amended (16 U.S.C. 470 et seq.). A federal agency may not conduct or sponsor, and a person is not required to respond to a collection of information unless it displays a valid OMB control number.

Estimated Burden Statement: Public reporting burden for this form is estimated to average 18.1 hours per response including the time for reviewing instructions, gathering and maintaining data, and completing and reviewing the form. Direct comments regarding this burden estimate or any aspect of this form to Keeper, National Register of Historic Places, 1849 "C" Street NW, Washington, DC 20240.

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National Register of Historic Places Continuation Sheet

Section 7 Page 1 of 10 Name of property Pu'u'ōpae Bridge County and State Kaua'i, Hawai'i

Narrative Description

(Describe the historic and current condition of the property on one or more continuation sheets.)

Pu'u'ōpae Bridge is a one lane, 48-foot long, single span structure that carries Pu'u'ōpae Road over Kalama Stream (a small stream approximately three feet deep and 12 feet wide) in the Kapa'a Homesteads 2nd Series. The height of the soffit of the bridge over the stream is about 11 feet. Built in 1915, it is the earliest bridge included in the *Historic Bridge Inventory: Island of Kauai* to be constructed beyond the island's circumferential Belt Road, in Kaua'i's homesteading lands.

Location. The bridge is in its original location. The placement of Pu'u'ōpae Road is shown on the Hawai'i Territory Survey map of "Kapaa Homesteads 2nd Series: Kapaa-Waipouli-Olohena, Island of Kauai," created in 1912 when the tract was subdivided in preparation for its settlement the following year. (See detail on Continuation Sheet, Section 10, Page 3.)

Design. Pu'u'ōpae Bridge was originally trussed, as evidenced by a small sketch included in correspondence regarding the construction of the bridge in 1915. The truss was removed in 1958, but the bridge's original, concrete-encased steel floor remains in place. According to the Spencer Mason *Historic Bridge Inventory: Island of Kauai*, "only this bridge and the 'Ele'ele Pedestrian Overpass [on the West side of Kaua'i] are classified as having steel girder floor systems on the State Bridge Inventory,"¹ and the Pu'u'ōpae Bridge predates 'Ele'ele by almost a quarter of a century. Pu'u'ōpae Bridge has been neither widened nor extended.

Setting. The setting has not changed substantially. Three houses have been built in the vicinity, on Kalama Road southwest of the bridge, but they have not altered the rural character of the area, which is defined by pastoral and agricultural lands sweeping from the east side of the bridge to the foot of Nounou (Sleeping Giant) Mountain in the distance.

Materials. The concrete deck, steel girder floor system and sections of the abutments are original materials. The endposts may be recycled parts of the nineteenth century Wailua River Bridge that was dismantled in 1919 and used for roads and bridges in the area. (See "Age," below.)

An archaeological survey of the bridge in 1996 provided the following description:

Remnants of a wooden form, probably used in the building of the deck were visible. The decks and steel girders were both encased in concrete and on the underside of the bridge, the concrete deck was imprinted with the grain of wood, left by the wooden form. A few of the original wood

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Narrative Description, Continued

pieces used in the form were found on both ends of the bridge adjacent to the abutments still attached to the underside of the deck. The steel soffits, railing supports and railing all seem to be part of one pre-engineered unit. At either end of the bridge on both sides of the abutments and adjacent to the railing are large pieces of steel which have been cut...²

Workmanship. The bridge is not in its original condition. Repair data located in the Kaua'i County Engineering office indicates that in 1958, two 48 foot I-beams were installed beneath the concrete and steel girder floor system for structural support. In order to imbed the I-beams, the headwalls above the stone abutments were broken and then repaired with concrete. While the plans indicate that the truss was still in place at the start of the project, it was probably removed after the I-beam support system was in place. (See detail on Continuation Sheet, Section 7, Page 5.) The railings were replaced in 2000, and galvanized **W** guardrails were substituted for the previous wood guardrails.

Feeling. There is a historic feeling to this bridge because of its narrow width, as well as its location in the back-country, still-agricultural/pastoral lands of Kapaa Homesteads 2nd Series.

Age. Written correspondence between the Territory of Hawai'i's Superintendent of Public Works, Charles R. Forbes, and Kauai County's Road Supervisor and Engineer, Joseph H. Moragne, establishes the bridge's period of construction as being between March and July 1915. An agreement for the Territory to pay the county \$800 to construct the bridge and the county to build it was approved by the Kauai Board of Supervisors on April 7, 1915.³ At the June 8, 1915 meeting of the Board of Supervisors, Moragne reported that construction was in progress; it had been completed by the time of the August report.⁴ (See Continuation Sheet, Section 7, pages 6-10.)

2. The end posts have the same dimensions and riveted construction as the top chords and western end posts of the Ōpaeka'a Road Bridge.⁵ It is known that when the 1890, Scottish-made Wailua Bridge was replaced in 1919, parts of the old bridge were "disposed along the side of the road awaiting removal to some needy spot where they may serve for small bridge trusses, coverings for culverts, etc. Some of them will probably be used on the homestead roads."⁶ Ōpaeka'a Road Bridge, one mile from the Pu'u'ōpae Bridge, was constructed from parts of the dismantled bridge. The County Road Supervisor's activities report of May 1920, eight months

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Narrative Description, Continued

after Wailua Bridge was dismantled, notes that small bridge repairs had been completed in the area: Since the end posts on Pu'u'ōpae Bridge are so similar to the 'Ōpaeka'a Road Bridge top chords and western end posts, they may also have come from the old Wailua Bridge.⁷ (Usable parts from the old bridge were still available as late as 1925, when chords from it were put into use as stringers in construction of the Kalama Bridge on Kamalu Rd., near the border between Wailua and Kapa'a Homesteads.)

Artistic Value. The *Historic Bridge Inventory: Island of Kauai* characterizes the bridge as “functional,” with little in the way of artistic details. However, it notes, the pattern formed by the rivets is unusual: “It is one of only three bridges on Kaua'i where riveted metal construction is visible.”⁸ The other two are 'Ōpaeka'a Road Bridge (National Register of Historic Places Site No. 30-08-9377) and the Hanalei River Bridge (National Register of Historic Places Site No. 30-03-736).

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Narrative Description, Continued

NOTES

1. Spencer Mason Architects, *Historic Bridge Inventory: Island of Kauai*, (Honolulu: Prepared for the State of Hawaii Department of Transportation Highways Division in cooperation with the U. S. Department of Transportation Federal Highway Administration, 1989), p. 217.
2. Tina Bushnell and Hallett H. Hammatt, "Archaeological Investigation of Pu'uopae (Kalama) Bridge in Wailua Homesteads, South Olohena, Ahupua'a Puna District, Kauai, Hawaii," (Study prepared by Cultural Surveys Hawaii, Inc. for Belt Collins and Associates, December 1996), p. 5.
3. "Blueprints showing data of the proposed Kapaa Homesteads Bridge #2 (Puuopae Rd) + com. Regarding agreement same to be constructed by Kauai County," records of the Kauai County Board of Supervisors, P.1150, April 7, 1915.
4. Some of the road names in the homesteads—including Pu'u'ōpae, Pu'upilo and Kalama—have, confusingly, changed since they were constructed. Pu'u'ōpae Rd. was originally U-shaped. It led south from Olohena then, after Kalama stream, climbed uphill on what is now called Kalama Rd. The section to the southwest that is now the continuation of Pu'u'ōpae was originally named Pu'upilo. In the two Board of Supervisors reports, Moragne mis-located the bridge on Pu'upilo, which intersected with the Pu'u'ōpae U about 40 yards from the bridge.
5. Spencer Mason Architects, p. 216.
6. "Wailua Bridge Finished," *The Garden Island*, September 23, 1919, p 1.
7. "Board of Supervisors Meet," *The Garden Island*, July 8, 1920, p. 4.
8. Spencer Mason Architects, p. 217.

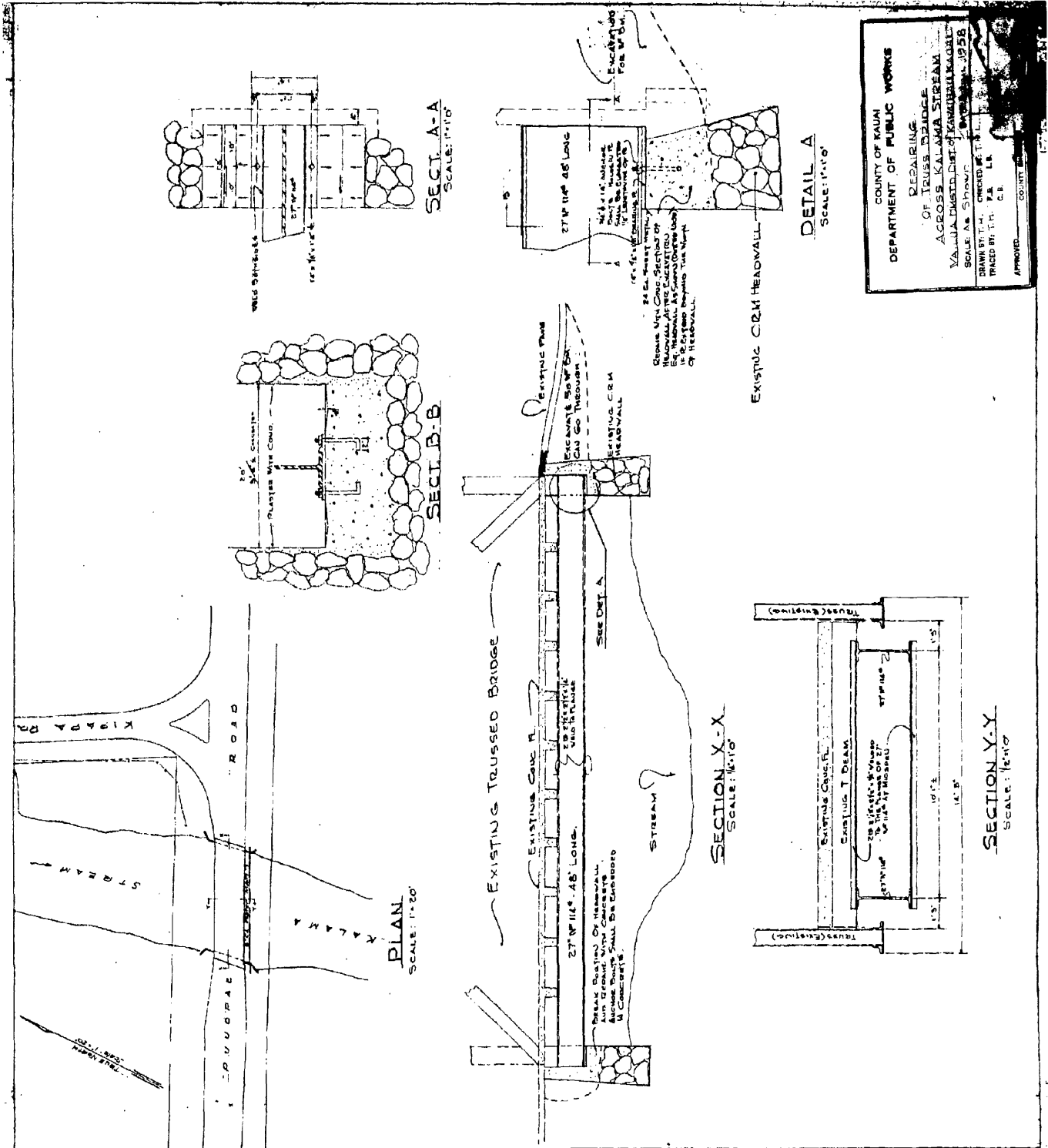
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Narrative Description, Continued

Kaua'i County Department of Public Works. "Repairing of Truss Bridge Across Kalama Stream, Wailua Hmstd, Dist. of Kawaihau, Kauai." 1958.



COUNTY OF KAUI
 DEPARTMENT OF PUBLIC WORKS
 REPAIRING
 OF TRUSS BRIDGE
 ACROSS KALAMA STREAM
 WAILUA DIST. DIST. OF KAWAIHAU, KAUI
 SCALE AS SHOWN
 DRAWN BY T.H. CHICKO
 TRACED BY T.H. CHICKO
 APPROVED: COUNTY OF KAUI

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Narrative Description, Continued

Kaua'i County Board of Supervisors records, P1150

Number P. 1150 Indexed

OFFICE

OF

BOARD OF SUPERVISORS COUNTY OF KAUA'I
TERRITORY OF HAWAII

*Blueprints showing data of
the proposed Hoopa. Hoanatahi
Bridge #2 (Pu'uopae Rd.) & conn.
regarding agreement made to
be constructed by Kauai County
Dated March 23, 1915*

Received by the Board May 5th 1915

Action taken Agreement accepted

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.....

.....

Vide Minute Book, Vol. 2 Page 161

Filed April 7th 1915

J. Mahini Kaunika
County Clerk, County of Kauai, T. H.

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Narrative Description, Continued

Kaua'i County Board of Supervisors records, P1150

CHAS. R. FORBES
~~J. W. CALDWELL~~
ASSOCIATE MEMBER A.S.C.E.
SUPERINTENDENT

Transcribed

TERRITORY OF HAWAII
DEPARTMENT OF PUBLIC WORKS
HONOLULU, T. H.

March 25, 1915

Mr. J. H. Moragne,
Lihue, Kauai.

Dear Sir:

KAPAA HOMESTEAD BRIDGE, NO. 2.

Replying to your letter of March 19th, I beg to submit herewith two blueprints of Plan No. 2219, showing some data regarding this proposed bridge. I also inclose an agreement showing that the Land Commissioner has set aside the sum of \$800.00; the work to be done under my direction.

I therefore turn over to you the expenditure of this fund, such expenditure to be made in connection with the county work on this bridge. This amount will be turned over to the County on the final completion and acceptance of the work.

Very truly yours,
Charles R. Forbes [signature]
Superintendent of Public Works

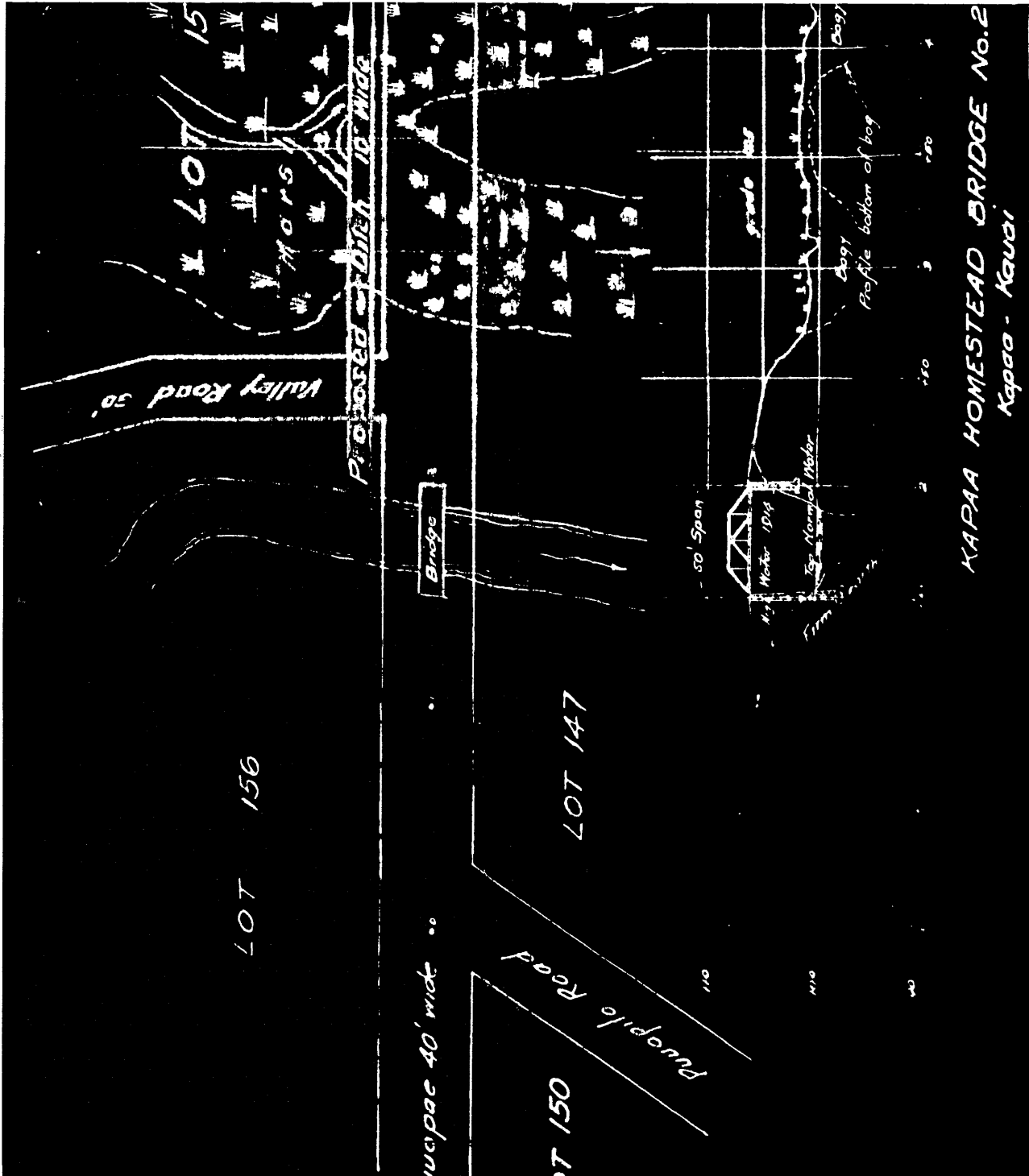
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Narrative Description, Continued

Kaua'i County Board of Supervisors records, P1150



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Narrative Description, Continued

Kaua'i County Board of Supervisors records, P1150

Transcribed

A G R E E M E N T

By the authority vested in me by Section 376,
Revised Laws, 1915, I hereby agree to set aside the sum of
Eight Hundred Dollars (\$800.00) to be paid to the County
of Kauai for the construction of a Bridge over the Puuopae
Road and just above the junction of this road with Valley
Road, providing this work is performed under the direction
of the Superintendent of Public Works and is finally
accepted
by him.

Joshua D. Tucker [signature]

Commissioner of Public Lands.

.....March 25.....1915.

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Narrative Description, Concluded

Kaua'i County Board of Supervisors records, P1150

OFFICE OF
COUNTY CLERK, COUNTY OF KAUAI.
TERRITORY OF HAWAII.

Transcribed

Lihue, April 10, 1915

charles R. Forbes, Esq.,
Superintendent, Public Works,
Honolulu, Hawaii.

Dear Sir :-

I beg to notify you that your offer under date of March 25th. last addressed to Mr. J.H. Morange [sic] our County Road Supervisor, of Eight Hundred Dollars (\$800.00) for the construction by the County of Kauai of a bridge, known as the Kapaa Homestead Bridge No. 2, the work to be performed under your direction, was duly received by the Honorable Board of Supervisors of the County of Kauai at its last regular monthly business meeting held here on the 7th inst., and that the same has been accepted by the said Board.

Very respectfully yours,

[unsigned]

County Clerk, County of Kauai.

By

Clerk.

**United States Department of the Interior
National Park Service**

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Section 8 Page 1 of 6 Name of property Pu'u'ōpae Bridge County and State Kaua'i, Hawai'i

Narrative Statement of Significance

(Explain the significance of the property on one or more continuation sheets.)

The construction of Pu'u'ōpae Bridge in 1915, two years after the Kapa'a Homesteads 2nd Series was opened, provided an important transportation conduit that contributed to the successful development of the homesteading lands on the east side of Kauai.

When Hawai'i became a territory of the United States in 1900, land ownership beyond that of the government's significant holdings was highly concentrated in the hands of a few large business interests and the individuals who controlled them. To encourage further settlement of family farmers in the islands, the government opened tracts of territorial land in Kapa'a, Kalāheo and elsewhere in Hawai'i.

The sale of public land as a strategy to increase Hawai'i's population of small, independent farmers was controversial. Most of the early territorial governors supported big sugar interests and showed "little faith in homesteading."⁹ Some governors were openly hostile to the movement: Lucius Pinkham (governor from November 29, 1913 to June 22, 1918) was reported as stating that he was "against the government lands being taken up by homesteaders. Homesteading is not a success; will never be a success and you know it damned well....All the cane lands of the government should remain in the ownership of the government and be leased to the sugar plantation."¹⁰ Critics charged that the government had diminished the chances for homesteading success after it traded its fertile lands to plantations in exchange for unproductive tracts not viable for farming and transferred vital water rights to them as well.¹¹

The dominant planter and business interests of the time were less than supportive of the homesteading philosophy themselves. Skeptics of their motives toward homesteaders accused the special interests of a land-grabbing scheme by which they advanced money to "marginal homesteaders" with the goal of becoming "the ultimate owners of every homestead the owner of which is so unfortunate as to be so easily gulled into the trap."¹² The possibility of gaining title to homesteaders' lands aside, sugar plantations were in a position to profit from cane-growing homesteaders through their monopoly of the milling and marketing processes, occasionally setting fees "so onesided and inequitable" that homesteaders across the territory protested.¹³

Despite the formidable opposition, some of Kaua'i's homesteading tracts began to show promise. In 1917 the chairman of the Chamber of Commerce Homestead Committee, E. W. Broadbent, reported that "the wisdom of the Homestead policy had been abundantly justified. On the Olohena-Waipouli tract there are ninety Homesteaders with 3140 acres who harvested this season 31,500 tons of cane, worth \$197,000, besides a considerable value in pines."¹⁴

The Olohena-Waipouli tract that Broadbent referenced was also known as Kapa'a Homesteads 2nd Series. It included 81 lots ranging in size from 17.27 acres to 41.32 acres. The lots were sold by

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Narrative Statement of Significance, Continued

lottery at the court house in Lihu'e on Saturday, June 28, 1913. Lottery winners were allowed up to three adjoining parcels, with a maximum of 80 acres. In an attempt to prevent abuse of the program by unscrupulous land speculators, winners were required to live on their property for given percentages of the first 10 years and to actively cultivate their lands. To sell or lease their property was restricted.

Although roads were indicated on the 1912 territorial survey map of the tract, they did not, in fact, exist when the Kapa'a lottery winners sought to move onto and begin farming their land. But the homesteaders were not a group to wait passively in the face of government inactivity. In August 1913, barely a month after the lottery, several of the new homesteaders petitioned the county for roads to "be constructed as soon as practicable for, unless the said roads, particularly 'Olohena', 'Puupilo', and 'Puupilo', are built, leveled, or ruts filled, it will be as it is now hard work for the homesteaders to bring their lumber for building purposes and their effects to their lands."¹⁵ The Board of Supervisors referred the matter to County Engineer and Road Supervisor Joseph H. Moragne for action, but a letter to *The Garden Island* six months later indicated that no discernable progress had been made: "As for roads, there are none, there are cattle trails all over this country and these were followed by oxcarts in hauling firewood from the mountains and there is nothing else in the way of roads there today."¹⁶

Water, too, was unavailable in Kapa'a Homesteads. In December 1913 the homesteaders petitioned the Supervisors "for water pipes to be laid for their use."¹⁷ The same *Garden Island* letter that complained of the lack of roads described the situation:

It is true that some of the lots have streams passing through them, but the water all belongs to the Makee Sugar Co., the Governor having sold the water rights to the Company before the tract was opened, the settlers having no right to use the water in any stream, even for household purposes, and they have been so notified by the company, unless they pay a monthly rate.¹⁸

Makee was in a potentially advantageous position with regard to the Kapa'a homesteaders for other reasons as well. Although many envisioned pineapple as the primary crop for the area, others championed sugar cane (ultimately both would be grown). Both Makee and Lihue Plantation competed to build a railroad into the homesteads—a struggle that Makee eventually won—to collect the farmers' cane and mill it at the company's factory.¹⁹

The Kapa'a homesteaders continued their efforts throughout the 1910s to advance their positions. One or more of them appeared several times before the Board of Supervisors to urge the county to build or repair roads in the area, especially Olohena Rd., which provided the lifeline between the homesteads and the Belt Road. Several joined to form an association in 1918 to

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Narrative Statement of Significance, Continued

“further the interests of the Kapaa homesteaders,” and the group successfully secured water for irrigation after sending a delegation to meet with territorial officials in Honolulu. Some homesteaders became active in the Kauai Chamber of Commerce as another forum in which they could seek to redress grievances.

Their achievement in convincing the local and territorial governments to build and maintain roads and bridges in the area was significant. The experience with Kapa'a undoubtedly helped convince officials that roads should be put in place before new homesteading lands in the adjoining Wailua 1st series were lotteried “so that homesteaders may drive up to their prospective front doors just as soon as they have selected the lots.”²⁰

Kapa'a Homesteads roads and bridges increased in importance after the Wailua Homesteads were opened, because Wailua roads did not flow towards the ocean. Instead, they were connected to the Kapa'a Homesteads roads—Pu'upilo, Pu'u'ōpae and Kamalu (which ran parallel to Pu'u'ōpae along the foot of Nounou Mountain). Olohena funneled both sets of homesteaders from their *mauka* lands to the Belt Road. Wailua Homesteads was not directly linked to the Belt Road until 1936, when Depression-era federal funding constructed the Ōpaeka'a Stream Bridge and extended Kuamo'o Road to the ocean. As a result, this network—including Pu'u'ōpae Bridge—remained important throughout the homesteading era.²¹

By the time the 31 lots in adjacent Wailua Homesteads 1st Series were lotteried in December 1919, at least some of the Kapa'a Homesteaders were farming successfully: *The Garden Island* reported in January of that year that Kapa'a homesteader E. M. Cheatham was employing 40 men to harvest his cane and that of his neighbors. But those were the glory days. During the next decade the dropping prices paid for sugar made small-scale cane growing increasingly unprofitable. By 1945 the Land Laws Revision Commission review of homesteading in Hawai'i reported that lot owners had “become landlords to corporate agricultural operators.”²² Its final report to the governor on December 31, 1946 declared that

in Honolaa, the majority of the homesteads patented suitable for cane are cultivated to cane by the adjoining plantation, either under lease or planting contract....A similar situation exists at Kapaa, Kauai, where the majority of the homesteads patented suitable for pineapples are cultivated to pineapples by the Hawaiian Fruit Packers, Ltd., or Hawaiian Canneries Co.²³

Many of the Kapa'a homesteaders turned to occupations other than farming. To name but three examples, Elmer Cheatham moved to Makaweli to run a store; his brother-in-law Rolland Israel (who had been optimistic enough about the future of homesteading that he added to his Kapa'a holdings by buying a lot in the Wailua tract when it was opened in 1919) became Game Warden;

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Narrative Statement of Significance, Continued

C. K. Amalu served as a judge. Gradually, much of the Wailua and Kapa'a Homesteads was rezoned for residential use. Today, the "bowl" of land adjacent to Pu'u'ōpae Bridge, bordered by Pu'u'ōpae, 'Ōpaeka'a, Kamalu and Olohena roads contains some of the only remaining area of significant agricultural acreage (nearly 400 acres) in the region.

Persons. In the early territorial period, homesteading lands were released by the government in an attempt to increase settlement opportunities for small, independent farmers, especially Hawaiians "who kept up constant pressure through their political leaders to recapture Hawaii's lands for themselves," and recruits whom "Americanizers," hoped would increase the percentage of Anglo Saxons in Hawai'i and strengthen the psychological bonds with the United States.²⁴ One of the requirements of the lottery winners in Kapa'a Homesteads 2nd Series was that they be American citizens or declare their intention of becoming one.

An example of the Americanizers' intent is evidenced by the reception E. M. Cheatham received when he moved to the Homesteads. Cheatham, who had been employed as manager of B. F. Ehlers & Company (predecessor to Liberty House department store, now Macy's) before acquiring his homesteading lands in Kapa'a, was described by the *Garden Island* as "a real good citizen of the capital city...a live-wire of the Honolulu Ad. Club and an important figure in other organizations at Honolulu."²⁵ Three months later, under the headline "Homesteaders of the Right Kind," he was described as a man with "business ability, capital and those things that make for good citizenship."²⁶

The history of the Kapa'a Homesteads, however, is not the product of a single Great Man who shaped the area but the collective story of a group of settlers who struggled to make a life as independent farmers for themselves and their families. The names of the 1913 Kapa'a Homesteads lottery winners hints at a predominance of the Hawaiian and Caucasian settlers who bought into the homesteading idea, but people from other backgrounds were represented as well: Lino, Contrades, Kauai, Hanohano, Kauai, Kainoa, Kelekoma, Booge, French, Miyashi, Souza, Reis, Wilson, Tracy, Johonnot, Silva, Konda, Nasahiga, Hepa, Reichelt, Soto, Cummings, Louis, Achuck, Cheatham, Livesey, Israel, Cook, Jensen, Ferreira, Victorino, Barreta, Rapoza, Aroong, Ohai, Waiwaiole, Mailehuna, Rodrigues, Amalu, Kaiu, Ventura, Kikaahu.²⁷

The actual designer of the bridge has not been established, although Joseph Moragne, who was responsible for much of the early territorial roadwork and bridge design, is definitely associated with its erection through his position as county road supervisor and engineer as well as his correspondence with Charles Forbes about the construction of the bridge.

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Narrative Statement of Significance, Continued

Summary. The Pu'u'ōpae Bridge is unusual because of its historical links to the development of the Kapa'a Homesteads, and the direct involvement of that community in pressuring the government to provide adequate stream crossings and roads in the area; its concrete-encased steel girder floor system; and its probable use of recycled parts from the 19th century Wailua Bridge. It remains a valuable historical resource for interpreting the too-little understood, non-plantation-related development of Territorial Hawai'i.

NOTES

9. Robert H. Horwitz et al., *Public Land Policy in Hawaii: An Historical Analysis* (Honolulu: Legislative Reference Bureau, University of Hawaii, 1969), p. 26.

10. "Mr. Pinkham and Homesteading," *The Garden Island*, April 4, 1917, p. 1.

11. "The People Heard at Meeting of the Chamber of Commerce," *The Garden Island*, October 9, 1917, p. 1.

12. "Land Law Question," *The Garden Island*, February 25, 1913, p. 1.

13. "Some Difference of Opinion Between Homesteaders and Planters," *The Garden Island*, August 13, 1918, p. 1. Also, "Homesteaders to Get Concessions," *The Garden Island*, August 27, 1918.

14. "The People Heard at Meeting of the Chamber of Commerce," *The Garden Island*, October 9, 1917, p. 1.

15. "Petition from Kapaa for construction of homestead roads in Kapaa mauka," records of the Kauai County Board of Supervisors, P895, August 6, 1913.

16. "Public Lands Question," *The Garden Island*, January 20, 1914, p. 4.

17. "Petition from Kapaa homesteaders for pipes to be laid at the homestead for their use," records of the Kauai County Board of Supervisors, P940 and 940a, December 10, 1913.

18. "Public Lands Question," *The Garden Island*, January 20, 1914, p. 4.

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Narrative Statement of Significance, Concluded

19. Several articles and editorials between 1914 and 1916 in *The Garden Island* address the two plantations' competition to build a railroad in the homesteads. They include: "Right of Way at Waipouli Tract, April 14, 1914, p. 1; "Makee Sugar Co. Will Build Road to Waipouli," November 17, 1914, p. 1; "Waipouli R. R. Is Now Assured," February 16, 1915, p. 1; "Mr. Forbes Over Proposed Road," March 9, 1915, p. 1; "Kauai Homesteading Threatened," April 4, 1916, p. 8.

20. "The Wailua Homesteads," *The Garden Island*, September 23, 1919, p. 1.

21. Spencer Mason Architects, *Historic Bridge Inventory: Island of Kauai*, p. 128.

22. George W. Luter, *Report on Homesteading in Hawaii: 1839-1961*, (Honolulu, Department of Land and Natural Resources, 1961), p. 20.

23. Ibid, p. 21.

24. Lawrence H. Fuchs, *Hawaii Pono: A Social History* (New York: Harcourt Brace Jovanovich, 1961), p. 251ff.

25. "To Settle On Kauai," *The Garden Island*, January 13, 1914, p. 1.

26. "Homesteaders of the Right Kind," *The Garden Island*, March 31, 1914, p. 1.

27. The names of the lot owners are included on the map of the "Hawaii Territory Survey, Walter E. Wall Surveyor, Kapaa Homesteads 2nd Series, Kapaa-Waipouli-Oloheua, Island of Kauai, HTS Plat 3016, Surveyed by S. W. Tay, March-June, 1912," which is located in the Kaua'i Historical Society archives.

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pp. 328-339.

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Wall, Walter E. "Hawaii Territory Survey: Kapaa Homesteads 2nd Series." HTS Plat 3016. 1912.

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_____. "Hawaii Territory Survey: Wailua Homesteads 1st Series." HTS Plat 3033. 1919.

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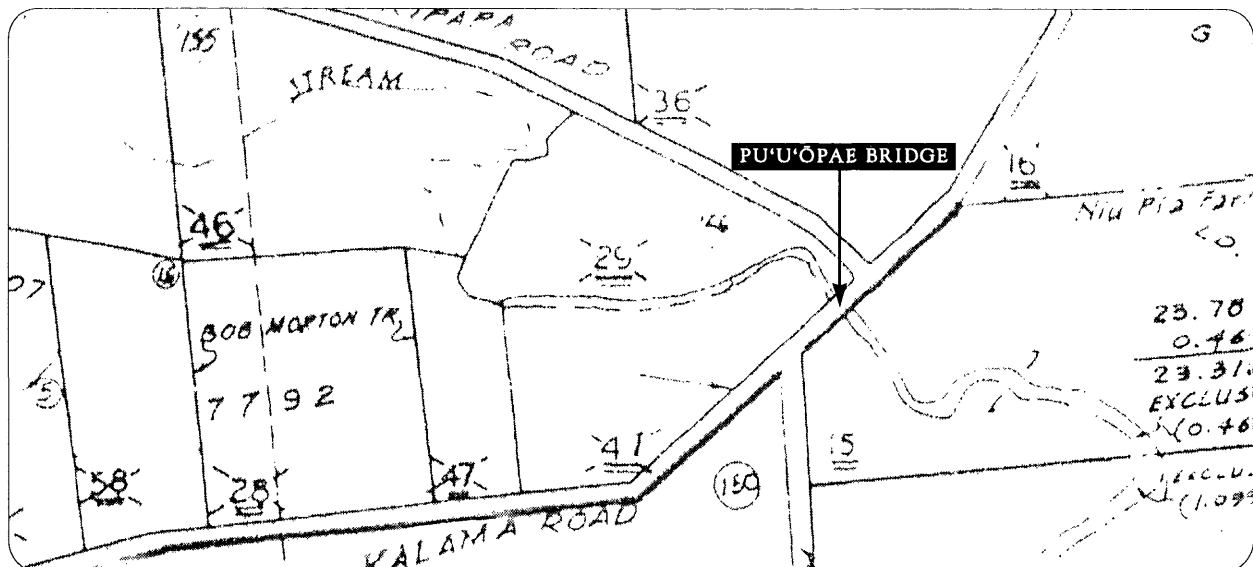
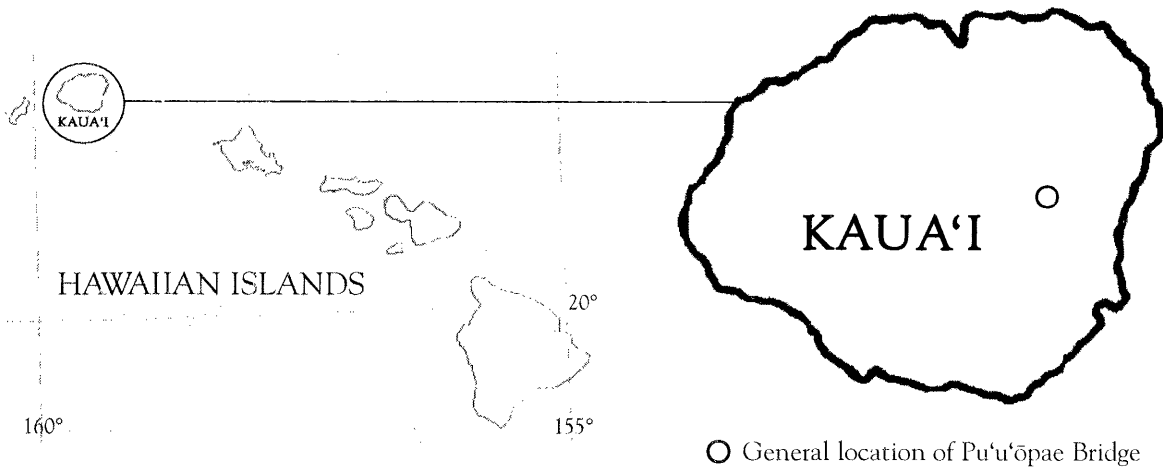
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Geographical Data

Verbal Boundary Description. Pu'u'opae Bridge is located in the *ahupua'a* of South Olohena in the Kawaihau District on the East side of the island of Kauai', Tax Map Key 4-4-002. The nominated property boundary encompasses only the bridge, its abutments and the ground upon which they stand, approximately 49 feet in length from endpost to endpost and 25.5 feet in width.

Boundary Justification. The nominated property consists exclusively of the bridge.



Above: Tax Maps Branch, State of Hawaii, Tax Map, Fourth District, 4-4-02 detail, Scale: 1" - 200'

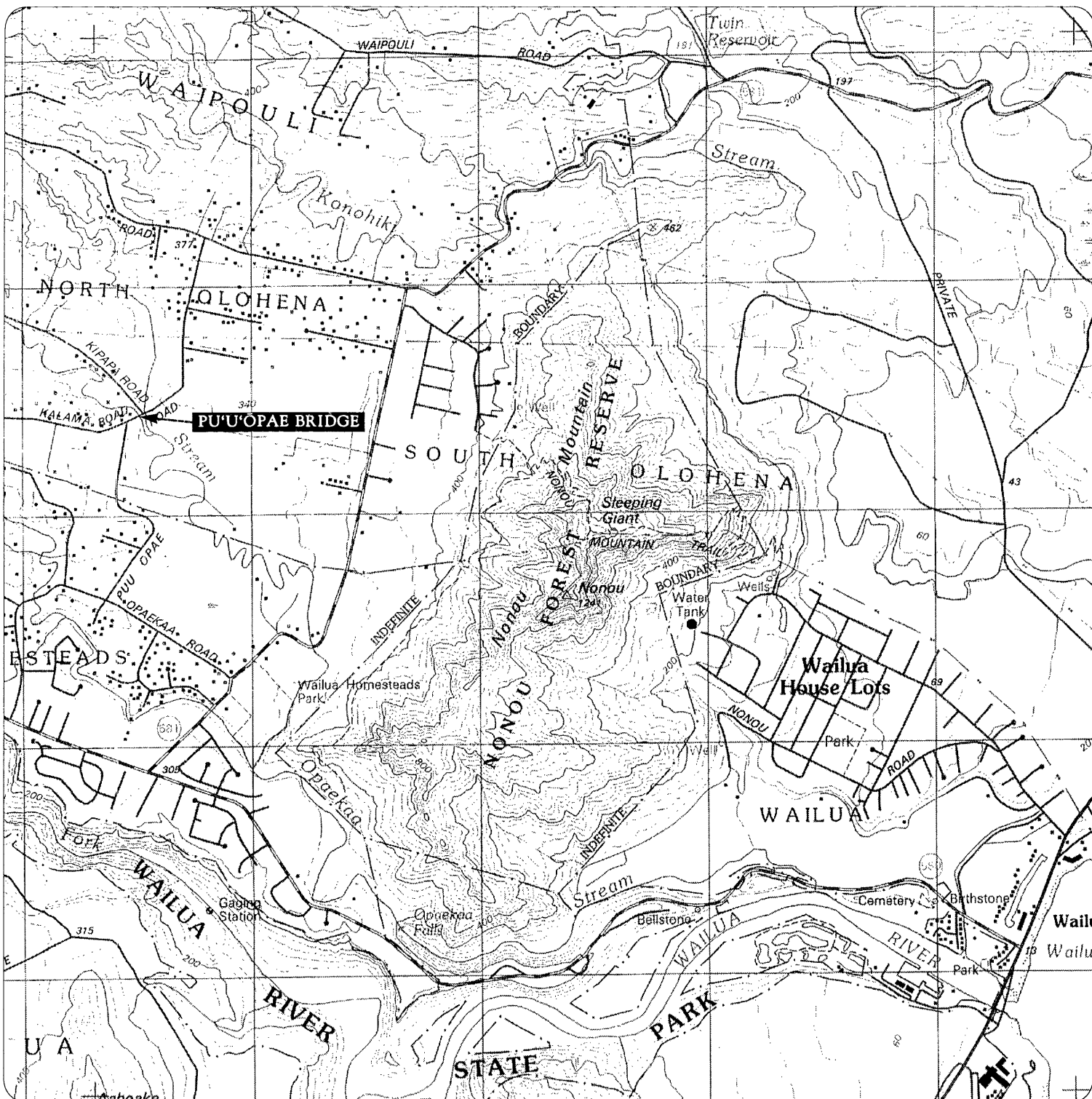
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Geographical Data

Detail of USGS Map, Kapaa Quadrangle, Hawaii-Kauai Co., 7.5-Minute Series (full map is enclosed).



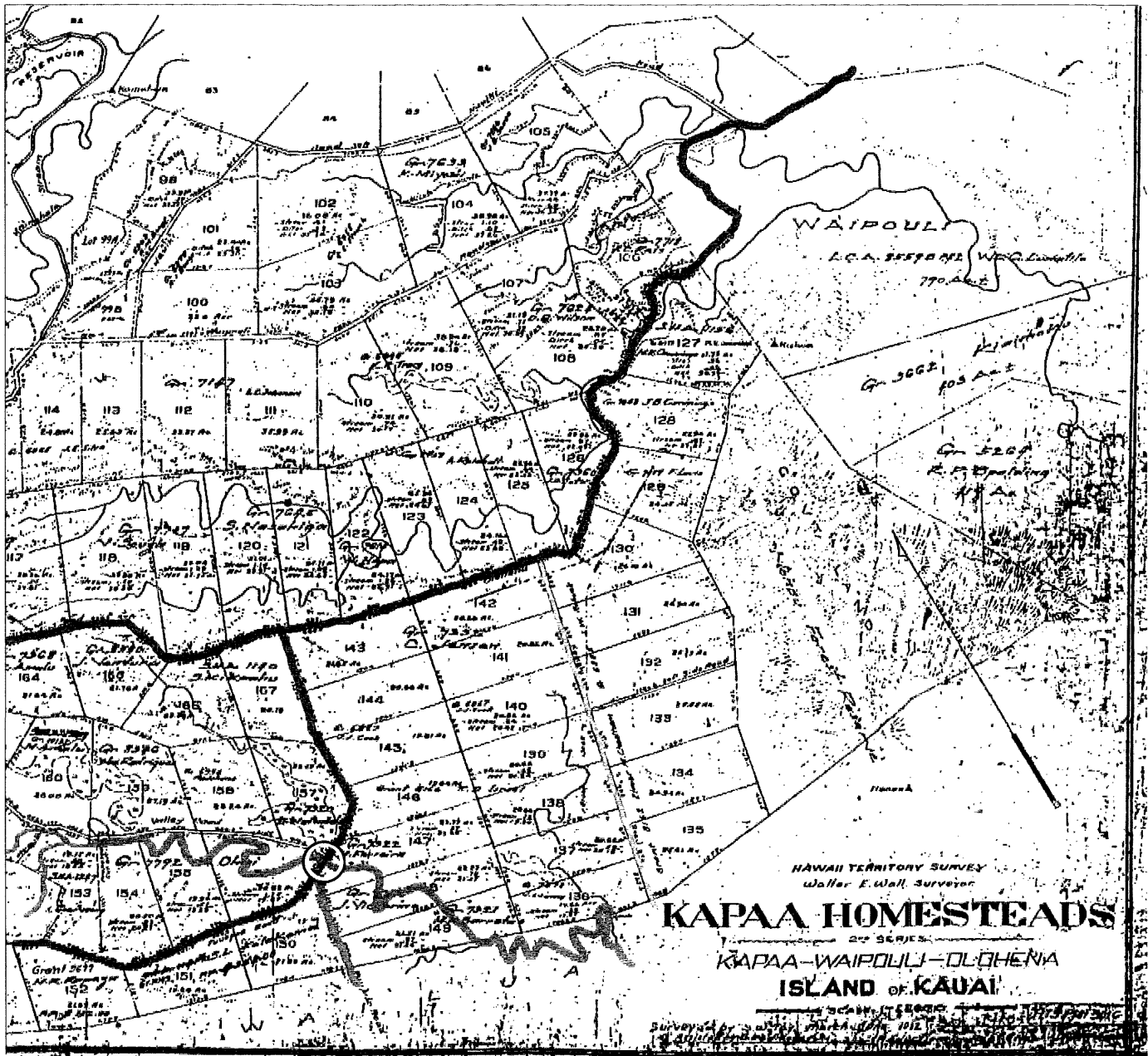
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Geographical Data

Map detail: Hawaii Territory Survey: Kapaa Homesteads 2nd Series. HTS Plat 3016. 1912



Pu'u'opae Bridge 
Kalama Stream 

Olohena Rd. 
Kamalu Rd. 

Pu'u'opae Rd. 
Pu'upilo Rd. 

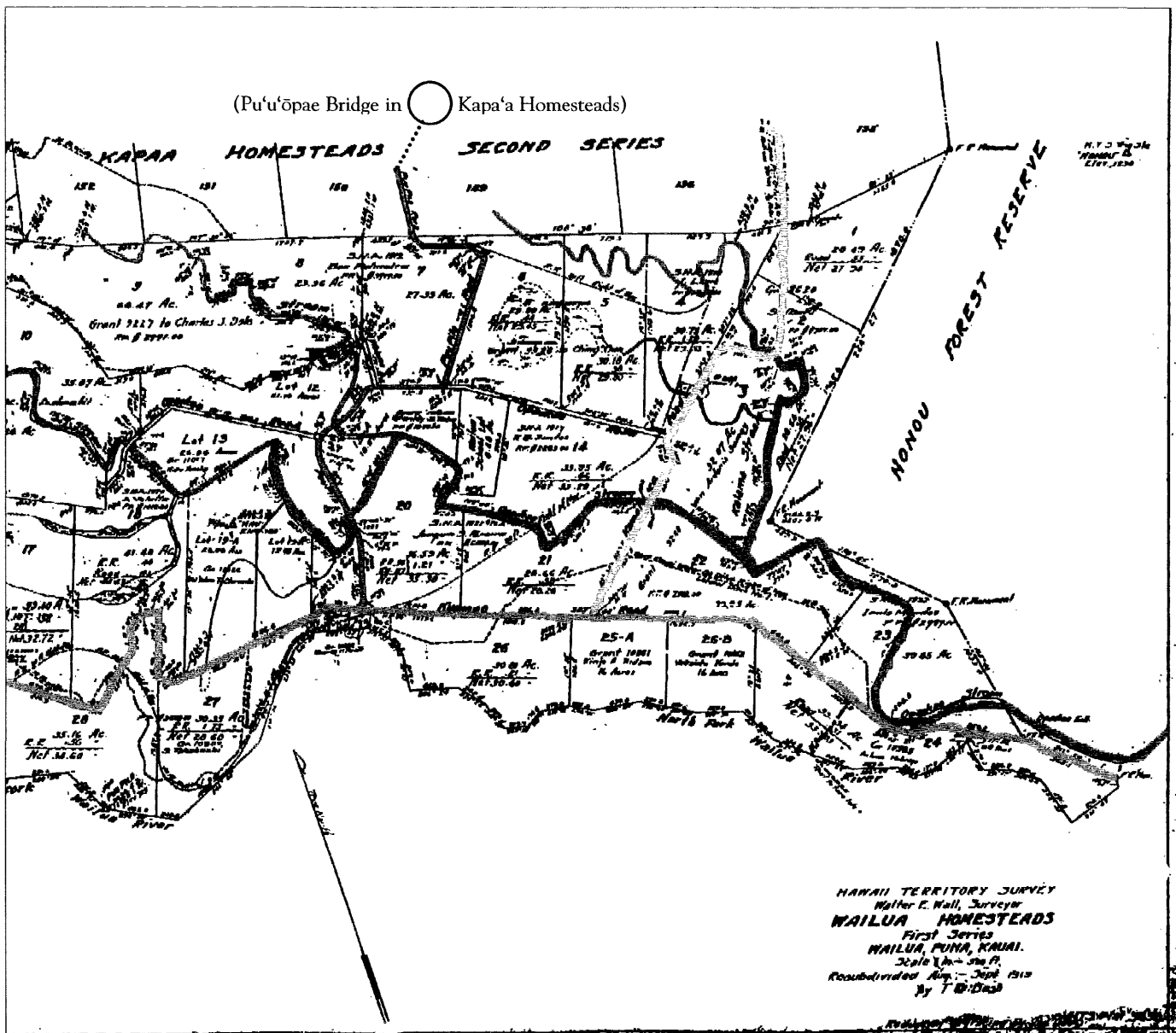
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Geographical Data

Map detail: Hawaii Territory Survey: Wailua Homesteads First Series, HTS Plat 3033. 1919



Pu'u'opae Bridge 
Kalamu Stream 

Kuamo'o Rd. 
Kamalu Rd. 

'Opaeaka'a Stream 
Pu'upilo Rd. 

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Additional Documentation Pg _ 1 of 3 _ Name of property Pu'u'ōpae Bridge _ County /State _ Kaua'i, Hawai'i

Photographs

Digital photos have been electronically transmitted to Hawai'i SHPD at its request.
All digital image data and negatives are located at the office of the nomination preparer.



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Approach to Pu'u'ōpae Bridge from Southwest



THOMAS NOYES

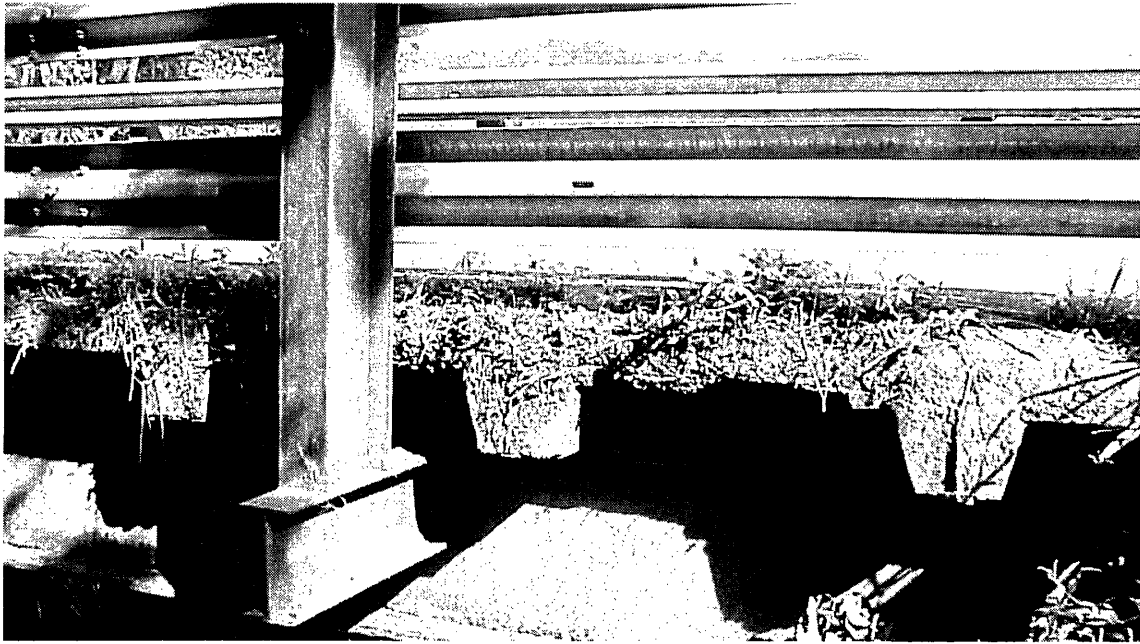
View of the bridge from Northwest side

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Additional Documentation Pg _ 2 of 3 _ Name of property Pu'u'ōpae Bridge _ County /State _ Kaua'i, Hawai'i

Photographs



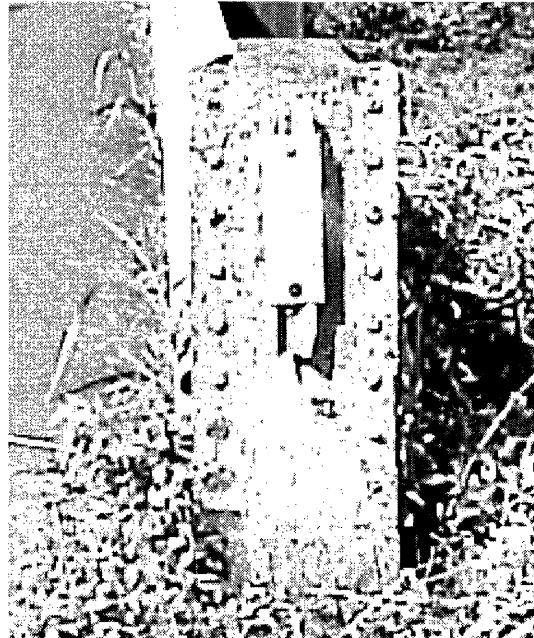
THOMAS NOYES

View from side, showing girder floor system



THOMAS NOYES

Detail: Girder



THOMAS NOYES

Detail: Endpost

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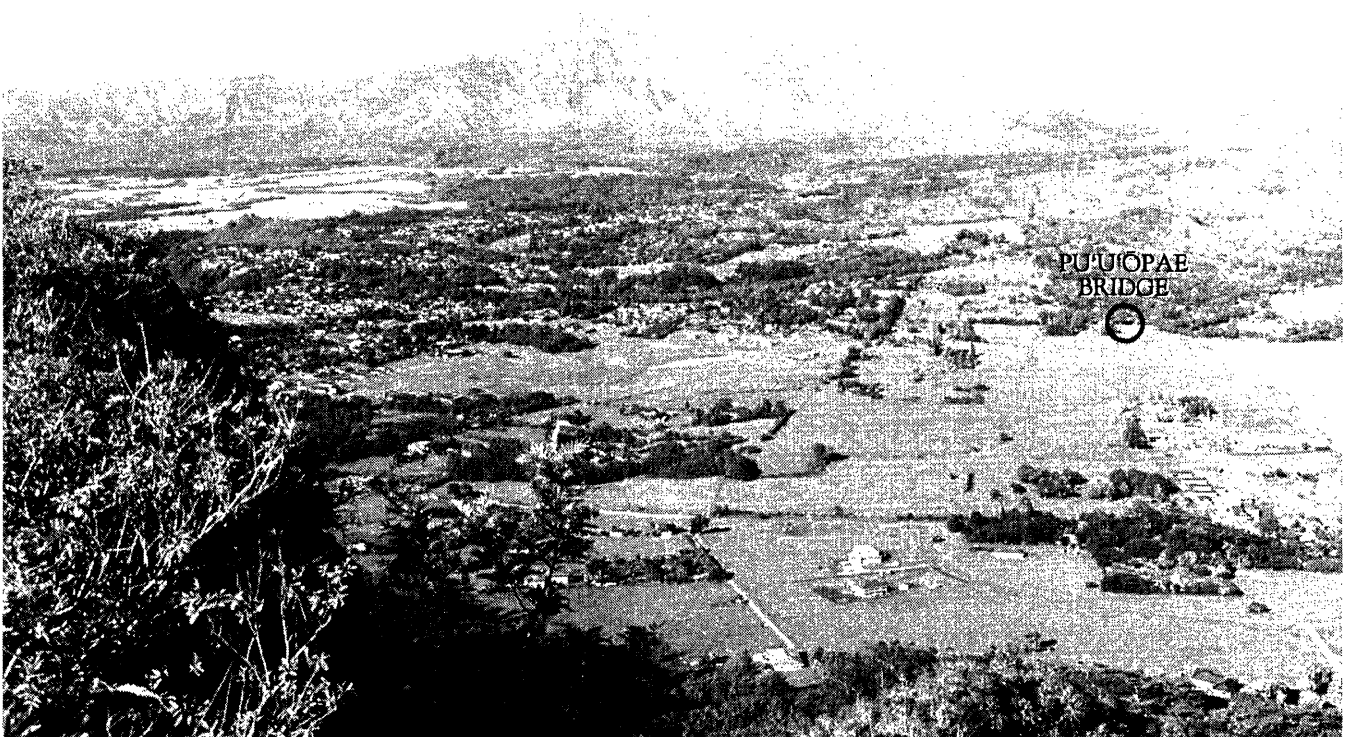
Additional Documentation Pg _ 3 of 3 _ Name of property Pu'u'ōpae Bridge _ County /State _ Kaua'i, Hawai'i

Photographs



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View East, towards Nounou, from bridge



PAT L GRIFFIN

View West, of Kapa'a and Wailua Homesteads from Nounou Mountain (Mt. Wai'ale'ale in distance)